

**STORM WATER POLLUTION PREVENTION PLAN (SWPPP)**

**AND**

**LARGE CONSTRUCTION NOTICE OF INTENT (LCNOI)**

**FOR**

**TAXIWAY B REHABILITATION**

**Greenville, Mississippi**

**Prepared for:**

**Mid-Delta Regional Airport Authority**

**166 Fifth Avenue, Suite 300**

**Greenville, MS 38703**

**June 2020**

**Prepared by:**



**Michael Baker Jr., Inc.**

**2113 Government Street, Suite D-3**

**Ocean Springs, MS 39564**

**(228) 818-2839 phone**

**(228) 875-5187 fax**

## **Rehabilitate Taxiway Site Information**

The proposed construction site lies within the airfield of the Mid-Delta Regional Airport in Greenville, Mississippi. The project will disturb approximately 31 acres, to rehabilitate Taxiway B from Taxiway D north to Runway 18L. The rehabilitation will consist of removing the existing paved shoulders and replacing them with grassing. Additionally, Taxiway B will be resurfaced with asphalt pavement and new airfield markings will be installed. The majority of the site is open and is covered with grass and pavement. Soils onsite consist of silty clays and clays. Currently, water from the site drains from west to east, enters an existing drainage system, and leaves the site to the northeast. From there, the water flows a short distance through natural and man-made drainages, emptying into the Mississippi River. The nearest receiving stream is Deer Creek. Deer Creek is not on the 303(d) list for siltation, turbidity or habitat alterations; therefore, additional controls that are warranted for a site discharging to listed receiving streams are not required.

## **Controls**

**Vegetative Controls:** Permanent grassing will be applied whenever grading operations are complete and additional construction operations will not impact the disturbed area. Permanent grassing will also be applied to all non-construction areas that show signs of excess erosion. Temporary grassing will be applied immediately to disturbed areas where land disturbing activities have ceased and will not resume for a period of fourteen (14) calendar days or more and permanent grass is not required. Topsoil will be stockpiled and used in areas that will be revegetated. Grass that will not compete with the grasses sown later for permanent cover shall be a quick-growing species (such as ryegrass, Italian ryegrass, or cereal grasses) suitable to the area providing a temporary cover. Grass that attracts wildlife will not be used on the project site. All areas receiving either temporary or permanent grass mixtures will receive an application of fertilizer and be protected with mulch. Mulches may be hay, straw, fiber mats, netting, bark, wood chips, or other suitable material that is reasonably clean and free of noxious weeds and deleterious materials. Mulches shall not create a wildlife attractant. Fertilizer shall be a standard commercial grade and shall conform to all federal and state regulations and to the standards of the Association of Official Agricultural Chemists. Vegetative buffer zones are undisturbed or planted vegetated areas that are between construction activities and water bodies, streets, drainage ditches, etc. The contractor will provide and maintain a 50-foot undisturbed natural buffer around waters of the United States; or provide and maintain an undisturbed natural buffer that is less than 50 feet and is supplemented by additional erosion and sediment controls, which in combination achieves the sediment load reduction equivalent to a 50-foot undisturbed natural buffer. Storm water will be directed to vegetated areas and maximize storm water infiltration to reduce pollutant discharges, unless infeasible.

**Structural Controls:** Perimeter controls consisting of 13,000 linear feet of silt fencing will encompass all grading operations during construction activities. Silt fence shall consist of

polymeric filaments which are formed into a stable network such that filaments retain their relative positions. Synthetic filter fabric shall contain ultraviolet ray inhibitors and stabilizers to provide a minimum of six months of expected usable construction life. Silt fence shall meet the requirements of ASTM D6461. Silt fencing will be placed well inside the right-of-way and along the edge of clearing limits to allow room for a backup fence if the first fence becomes full. The silt fences will be installed opposite erodible areas, such as newly graded fill slopes and adjacent to streams and channels. Sediment barriers consisting of straw wattle will be installed around any storm drain inlets located within and adjacent to the disturbance areas on site. In the event that wattles cannot be secured using wooden stakes, sandbags may be used to secure the wattles in place. A stabilized construction entrance will be installed prior to the intersection of any public road to help reduce the vehicular tracking of sediments on the roads. The stabilized areas will consist of a layer of aggregate to a depth of at least six (6) inches above a geotextile filter fabric for a width of at least twenty-four (24) feet and a length of at least one hundred (100) feet.

**Housekeeping Practices:** Trucks will be washed or rinsed prior to leaving the project site to prevent sediment transport off-site. Any detergents that may be used to wash the trucks will not be discharged into any waters. The contractor will be responsible for storing all chemicals, paints, solvents, fertilizers, pesticides, herbicides, detergents, and other potentially toxic materials in a secured approved location. The contractor will be responsible for preventing the deposition of fuels and oils within the project site at all times. Any leaks or spills identified will be immediately reported and addressed. All materials spilled, dropped, or tracked onto any public roads, including the stabilized construction entrance, will be removed daily or more frequently. Materials and equipment necessary for a petroleum spill cleanup must be kept onsite and in storage areas. Maintenance includes removal of sediment beginning when sediment accumulation reaches one-third ( $1/3$ ) the capacity or height of the structure and never allowing for sediment to accumulate more than one-half ( $1/2$ ) the volume or height of the ditch check structure. Adequately maintained portable sanitary facilities will be provided for construction workers. Waste receptacles and regular collection of waste will be provided.

**Post-Construction/Storm Water Management Measures:** At the time that satisfactory vegetation has been established, all temporary erosion control BMPs shall be removed from the site. All erosion and sediment control BMPs shall be removed by the contractor once the site is stabilized.

### **Implementation Sequence**

Major construction activities will begin when all permits have been obtained and work is authorized. The project sequence will be as follows: 1) Install stabilized construction entrance; 2) Install perimeter control measures; 3) Protect all inlets within the disturbed areas; 4) Perform stripping and stockpile topsoil; 5) Temporarily seed stockpiles and applicable disturbed areas; 6) Complete final grading; 7) Establish permanent grassing; and 8) After the site is stabilized, remove all temporary sediment and erosion control measures.

### **Maintenance Plan**

The project area will be inspected frequently within the first six weeks of planting to see if grass stands are uniform and dense and to assure that appropriate moisture levels are maintained. Additional vegetation will be employed where determined necessary by actual site conditions. Erosion control measures will be inspected daily and after each rain. Damaged areas will be corrected as soon as possible, but not to exceed 24 hours of the inspection unless prevented by unsafe weather conditions. Damaged areas should be repaired, fertilized, seeded, and mulched if full implementation of the approved plan does not provide for effective erosion control. Additional erosion and sediment control measures shall be implemented to control or treat the sediment source. All BMPs shall be regularly maintained. Sediment shall be removed from BMPs when the height of the sediment is one-third of the height of the BMP. The stabilized construction entrance will be maintained in a condition to allow it to perform its function to prevent offsite tracking, and the stabilized construction entrance shall be rinsed when necessary to remove accumulated mud.

**Appendix A**  
**Large Construction Notice of Intent**  
**(LCNOI) Form**



MISSISSIPPI DEPARTMENT OF  
ENVIRONMENTAL QUALITY

**MISSISSIPPI DEPARTMENT OF  
ENVIRONMENTAL QUALITY (MDEQ)  
Large Construction Storm Water General Permit  
NPDES Permit MSR10**

# **LARGE CONSTRUCTION FORMS PACKAGE**

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**These standard forms are used to apply for permit coverage under the Large Construction Storm Water General Permit and for submittals and record keeping required by permit conditions after coverage has been granted. The forms are on our website at [www.deq.state.ms.us/MDEQ.nsf/page/epd\\_epdgeneral](http://www.deq.state.ms.us/MDEQ.nsf/page/epd_epdgeneral). Required information can be completed on screen, printed and signed.**



MISSISSIPPI DEPARTMENT OF  
ENVIRONMENTAL QUALITY

## **LARGE CONSTRUCTION NOTICE OF INTENT (LCNOI) FOR COVERAGE UNDER THE LARGE CONSTRUCTION STORM WATER GENERAL NPDES PERMIT**

### **INSTRUCTIONS**

The Large Construction Notice of Intent (LCNOI) is for coverage under the Large Construction General Permit for land disturbing activities of five (5) acres or greater; or for land disturbing activities, which are part of a larger common plan of development or sale that are initially less than five (5) acres but will ultimately disturb five (5) or more acres. Applicant must be the owner or operator. For construction activities, the operator is typically the prime contractor. The owner(s) of the property and the prime contractor associated with regulated construction activity on the property have joint and severable responsibility for compliance with the Large Construction Storm Water General Permit MSR10.

If the company seeking coverage is a corporation, a limited liability company, a partnership, or a business trust, attach proof of its registration with the Mississippi Secretary of State and/or its Certificate of Good Standing. This registration or Certificate of Good Standing must be dated within twelve (12) months of the date of the submittal of this coverage form. Coverage will be issued in the company name as it is registered with the Mississippi Secretary of State.

Completed LCNOIs should be filed at least thirty (30) days prior to the commencement of construction. Discharge of storm water from large construction activities without written notification of coverage is a violation of state law.

Submittals with this LCNOI must include:

- A site-specific Storm Water Pollution Prevention Plan (SWPPP) developed in accordance with ACT5 of the General Permit
- A detailed site-specific scaled drawing showing the property layout and the features outlined in ACT5 of the General Permit
- A United States Geological Survey (USGS) quadrangle map or photocopy, extending at least one-half mile beyond the facility property boundaries with the site location and outfalls outlined or highlighted. The name of the quadrangle map must be shown on all copies. Quadrangle maps can be obtained from the MDEQ, Office of Geology at 601-961-5523.

Additional submittals may include the following, if applicable:

- Appropriate Section 404 documentation from U.S. Army Corps of Engineers
- Appropriate documentation concerning future disposal of sanitary sewage and sewage collection system construction
- Appropriate documentation from the MDEQ Office of Land & Water concerning dam construction and low flow requirements
- Approval from County Utility Authority in Hancock, Harrison, Jackson, Pearl River and Stone Counties

**ALL QUESTIONS MUST BE ANSWERED (Answer "NA" if the question is not applicable)**

MSR10 \_ \_ \_ \_

(NUMBER TO BE ASSIGNED BY STATE)

APPLICANT IS THE: ☒ OWNER ☐ PRIME CONTRACTOR

## OWNER CONTACT INFORMATION

OWNER CONTACT PERSON: Samuel Washington  
OWNER COMPANY LEGAL NAME: Greenville Mid-Delta Regional Airport  
OWNER STREET OR P.O. BOX: 166 Fifth Avenue, Suite 300  
OWNER CITY: Greenville STATE: Mississippi ZIP: 38703  
OWNER PHONE #: (662) 334-3121 OWNER EMAIL: SWashington@greenvillems.org

## PRIME CONTRACTOR CONTACT INFORMATION

PRIME CONTRACTOR CONTACT PERSON: \_\_\_\_\_  
PRIME CONTRACTOR COMPANY LEGAL NAME: \_\_\_\_\_  
PRIME CONTRACTOR STREET OR P.O. BOX: \_\_\_\_\_  
PRIME CONTRACTOR CITY: \_\_\_\_\_ STATE: \_\_\_\_\_ ZIP: \_\_\_\_\_  
PRIME CONTRACTOR PHONE #: ( ) \_\_\_\_\_ PRIME CONTRACTOR EMAIL: \_\_\_\_\_

## FACILITY SITE INFORMATION

FACILITY SITE NAME: Mid-Delta Regional Airport  
FACILITY SITE ADDRESS (If the physical address is not available, please indicate the nearest named road. For linear projects indicate the beginning of the project and identify all counties the project traverses.)  
STREET: 166 5th Street  
CITY: Greenville STATE: MS COUNTY: Washington ZIP: 38703  
FACILITY SITE TRIBAL LAND ID (N/A If not applicable): N/A  
LATITUDE: 33 degrees 29 minutes 0.40 seconds LONGITUDE: 90 degrees 58 minutes 55.64 seconds  
LAT & LONG DATA SOURCE (GPS (Please GPS Project Entrance/Start Point) or Map Interpolation): GPS (Google Earth)  
TOTAL ACREAGE THAT WILL BE DISTURBED <sup>1</sup>: 31  
IS THIS PART OF A LARGER COMMON PLAN OF DEVELOPMENT? YES ☐ NO ☒  
IF YES, NAME OF LARGER COMMON PLAN OF DEVELOPMENT: N/A  
AND PERMIT COVERAGE NUMBER: MSR10 \_ \_ \_ \_  
ESTIMATED CONSTRUCTION PROJECT START DATE: 2020-07-01  
YYYY-MM-DD  
ESTIMATED CONSTRUCTION PROJECT END DATE: 2021-01-31  
YYYY-MM-DD  
DESCRIPTION OF CONSTRUCTION ACTIVITY: The proposed activity is to rehabilitate Taxiway B from Taxiway D north to Runway 18L.  
PROPOSED DESCRIPTION OF PROPERTY USE AFTER CONSTRUCTION HAS BEEN COMPLETED:  
The property use would not change after construction.  
SIC Code 4 5 8 1 NAICS Code 4 8 1 1 1 1

NEAREST NAMED RECEIVING STREAM: Deer Creek

IS RECEIVING STREAM ON MISSISSIPPI'S 303(d) LIST OF IMPAIRED WATER BODIES? (The 303(d) list of impaired waters and TMDL stream segments may be found on MDEQ's web site: [http://www.deq.state.ms.us/MDEQ.nsf/page/TWB\\_Total\\_Maximum\\_Daily\\_Load\\_Section](http://www.deq.state.ms.us/MDEQ.nsf/page/TWB_Total_Maximum_Daily_Load_Section)) YES ☐ NO ☒

HAS A TMDL BEEN ESTABLISHED FOR THE RECEIVING STREAM SEGMENT? YES ☐ NO ☒

ARE THERE RECREATIONAL STREAMS, PRIVATE/PUBLIC PONDS OR LAKES WITHIN ½ MILE DOWNSTREAM OF PROJECT BOUNDARY THAT MAY BE IMPACTED BY THE CONSTRUCTION ACTIVITY? YES ☐ NO ☒

EXISTING DATA DESCRIBING THE SOIL (for linear projects please describe in SWPPP):

Nearly level, somewhat poorly drained to somewhat excessively drained silt loams and sandy loams of old natural levees: Dundee-Bosket-Beulah-Souva

WILL FLOCCULANTS BE USED TO TREAT TURBIDITY IN STORM WATER? YES ☐ NO ☒

IF YES, INDICATE THE TYPE OF FLOCCULANT.

☐ ANIONIC POLYACRYLAMIDE (PAM)

☐ OTHER \_\_\_\_\_

IF YES, DOES THE SWPPP DESCRIBE THE METHOD OF INTRODUCTION, THE LOCATION OF INTRODUCTION AND THE LOCATION OF WHERE FLOCCULATED MATERIAL WILL SETTLE? YES ☐ NO ☐

<sup>1</sup>Acreage for subdivision development includes areas disturbed by construction of roads, utilities and drainage. Additionally, a housesite of at least 10,000 ft<sup>2</sup> per lot (entire lot, if smaller) shall be included in calculating acreage disturbed.

**DOCUMENTATION OF COMPLIANCE WITH OTHER REGULATIONS/REQUIREMENTS**  
COVERAGE UNDER THIS PERMIT WILL NOT BE GRANTED UNTIL ALL OTHER REQUIRED  
MDEQ PERMITS AND APPROVALS ARE SATISFACTORILY ADDRESSED

**IS LCNOI FOR A FACILITY THAT WILL REQUIRE OTHER PERMITS?**

YES ☐

NO ☒

**IF YES, CHECK ALL THAT APPLY:**    ☐ AIR        ☐ HAZARDOUS WASTE        ☐ PRETREATMENT  
   ☐ WATER STATE OPERATING        ☐ INDIVIDUAL NPDES        ☐ OTHER: \_\_\_\_\_

**IS THE PROJECT REROUTING, FILLING OR CROSSING A WATER CONVEYANCE OF ANY KIND? (If yes, contact the U.S. Army Corps of Engineers' Regulatory Branch for permitting requirements.)**        YES ☐        NO ☒

**IF THE PROJECT REQUIRES A CORPS OF ENGINEER SECTION 404 PERMIT, PROVIDE APPROPRIATE DOCUMENTATION THAT:**

- The project has been approved by individual permit, or
- The work will be covered by a nationwide permit and NO NOTIFICATION to the Corps is required, or
- The work will be covered by a nationwide or general permit and NOTIFICATION to the Corps is required

**IS A LAKE REQUIRING THE CONSTRUCTION OF A DAM BEING PROPOSED?**        YES ☐        NO ☒  
(If yes, provide appropriate approval documentation from MDEQ Office of Land and Water, Dam Safety.)

**IF THE PROJECT IS A SUBDIVISION OR A COMMERCIAL DEVELOPMENT, HOW WILL SANITARY SEWAGE BE DISPOSED? Check one of the following and attach the pertinent documents.**

- ☐ Existing Municipal or Commercial System. Please attach plans and specifications for the collection system and the associated "Information Regarding Proposed Wastewater Projects" form or approval from County Utility Authority in Hancock, Harrison, Jackson, Pearl River and Stone Counties. If the plans and specifications can not be provided at the time of LCNOI submittal, MDEQ will accept written acknowledgement from official(s) responsible for wastewater collection and treatment that the flows generated from the proposed project can and will be transported and treated properly. The letter must include the estimated flow.
- ☐ Collection and Treatment System will be Constructed. Please attach a copy of the cover of the NPDES discharge permit from MDEQ or indicate the date the application was submitted to MDEQ (Date: \_\_\_\_\_.)
- ☐ Individual Onsite Wastewater Disposal Systems for Subdivisions Less than 35 Lots. Please attach a copy of the Letter of General Acceptance from the Mississippi State Department of Health or certification from a registered professional engineer that the platted lots should support individual onsite wastewater disposal systems.
- ☐ Individual Onsite Wastewater Disposal Systems for Subdivisions Greater than 35 Lots. A determination of the feasibility of installing a central sewage collection and treatment system must be made by MDEQ. A copy of the response from MDEQ concerning the feasibility study must be attached. If a central collection and wastewater system is not feasible, then please attach a copy of the Letter of General Acceptance from the State Department of Health or certification from a registered professional engineer that the platted lots should support individual onsite wastewater disposal systems.

**INDICATE ANY LOCAL STORM WATER ORDINANCE WITH WHICH THE PROJECT MUST COMPLY:**

\_\_\_\_\_  
\_\_\_\_\_

I certify under penalty of law that this document and all attachments were prepared under my direction or supervision in accordance with a system designed to assure that qualified personnel properly gathered and evaluated the information submitted. Based on my inquiry of the person or persons who manage the system, or those persons directly responsible for gathering the information, the information submitted is, to the best of my knowledge and belief, true, accurate and complete. I am aware that there are significant penalties for submitting false information, including the possibility of fine and imprisonment for knowing violations.



Signature of Applicant<sup>1</sup> (owner or prime contractor)

06/26/2020

Date Signed

Samuel Washington

Printed Name<sup>1</sup>

Airport Director

Title

<sup>1</sup>This application shall be signed as follows:

- For a corporation, by a responsible corporate officer.
- For a partnership, by a general partner.
- For a sole proprietorship, by the proprietor.

For a municipal, state or other public facility, by principal executive officer, mayor, or ranking elected official

Please submit the LCNOI form to:

Chief, Environmental Permits Division  
MS Department of Environmental Quality, Office of Pollution Control  
P.O. Box 2261  
Jackson, Mississippi 39225

# PRIME CONTRACTOR CERTIFICATION

## LARGE CONSTRUCTION GENERAL PERMIT

Coverage No. MSR10 \_\_\_\_\_ County \_\_\_\_\_  
(Fill in your Certificate of Coverage Number and County)



By completing and submitting this form to MDEQ, the prime contractor is certifying that (1) they have operational control over the erosion and sediment control specifications (including the ability to make modifications to such specifications) or (2) they have day-to-day operational control of those activities at the site necessary to ensure compliance with the SWPPP and applicable permit conditions.

The owner(s) of the property and the prime contractor associated with regulated construction activity on the property have joint and severable responsibility for compliance with the permit. Notwithstanding any permit condition to the contrary, the coverage recipient and any person who causes pollution of waters of the state or places waste in a location where they are likely to cause pollution of any waters of the state shall remain responsible under applicable federal and state laws and regulations and applicable permits.

### PRIME CONTRACTOR INFORMATION

PRIME CONTRACTOR CONTACT PERSON: \_\_\_\_\_ PHONE NUMBER: ( ) \_\_\_\_\_

PRIME CONTRACTOR COMPANY: \_\_\_\_\_

PRIME CONTRACTOR STREET (P.O. BOX): \_\_\_\_\_

PRIME CONTRACTOR CITY: \_\_\_\_\_ STATE: \_\_\_\_\_ ZIP: \_\_\_\_\_

E-MAIL ADDRESS: \_\_\_\_\_

### OWNER INFORMATION

OWNER CONTACT PERSON: Samuel Washington PHONE NUMBER: (662) 334-3121

OWNER COMPANY NAME: Greenville Mid-Delta Regional Airport

### PROJECT INFORMATION

PROJECT NAME: Taxiway B Rehabilitation Project

DESCRIPTION OF CONSTRUCTION ACTIVITY: The proposed project is to rehabilitate Taxiway B from Taxiway D north to Runway L. The rehabilitation will consist of removing the existing paved shoulders and replacing them with grassing. Additionally, Taxiway B will be resurfaced with new asphalt pavement and new airfield markings will be installed.

PHYSICAL SITE ADDRESS (If the physical address is not available indicate the nearest named road. For linear projects, indicate the beginning of the project and identify all counties the project traverses.)

STREET: 166 Fifth Avenue, Suite 300

CITY: Greenville COUNTY: Washington

I certify that I am the prime contractor for this project and will comply with all the requirements in the above referenced general NPDES permit. I further certify under penalty of law that this document and all attachments were prepared under my direction or supervision in accordance with a system designed to assure that qualified personnel properly gathered and evaluated the information submitted. Based on my inquiry of the person or persons who manage the system, or those persons directly responsible for gathering the information, the information submitted is, to the best of my knowledge and belief, true, accurate and complete. I am aware that there are significant penalties for submitting false information, including the possibility of fine and imprisonment for knowing violations.

Samuel Washington  
Prime Contractor Signature<sup>1</sup>

Samuel Washington  
Printed Name<sup>1</sup>

06/26/2020  
Date Signed  
Airport Director  
Title

<sup>1</sup>This application shall be signed as follows:

- For a corporation, by a responsible corporate officer.
- For a partnership, by a general partner.
- For a sole proprietorship, by the proprietor.
- For a municipal, state or other public facility, by principal executive officer, mayor, or ranking elected official.

This Prime Contractors Certification form shall be submitted to:

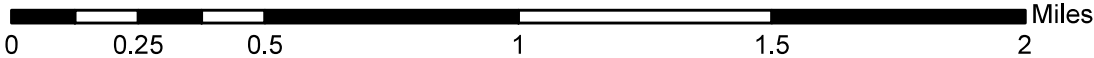
Chief, Environmental Permits Division  
MS Department of Environmental Quality, Office of Pollution Control  
P.O. Box 2261  
Jackson, Mississippi 39225

## **Appendix B**

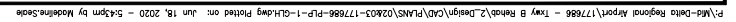
### **General Location Map**



Mid-Delta Regional Airport  
Taxiway B Rehabilitation  
General Location Map  
Greenville, MS  
April 2020



**Appendix C**  
**Erosion Control Plans & Detail Sheet**





**Michael Baker**  
INTERNATIONAL  
2113 Government Street, Suite D-3  
Ocean Springs, Mississippi  
Phone: (228) 819-2539 • [MBI@INTERNET.COM](mailto:MBI@INTERNET.COM)

Designer: KGP  
Technician: KGP  
Checked by: JJM  
Project Number: 11776866

[illegible]

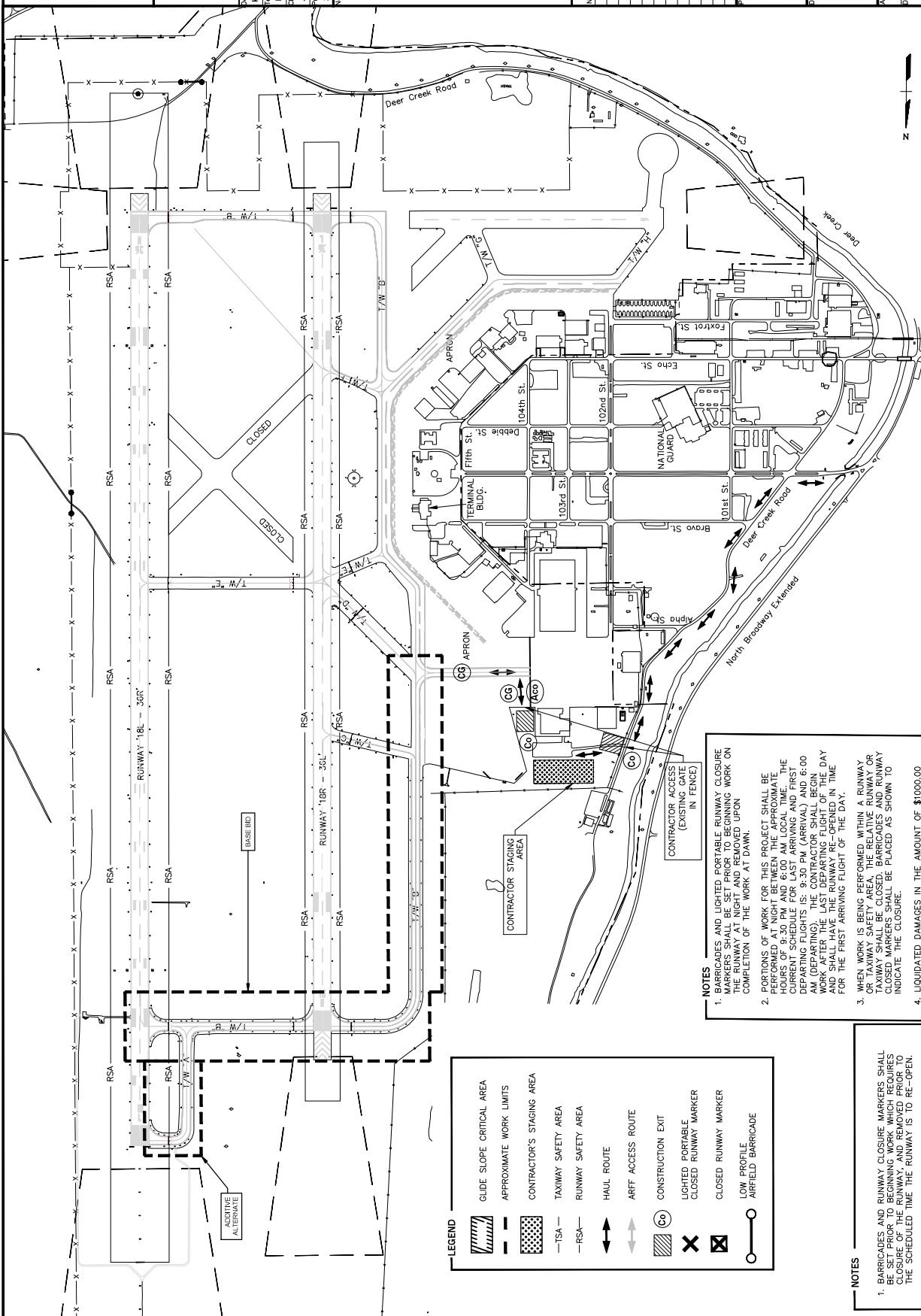
Project Name: **MID-DELTA INTERNATIONAL  
AIRPORT  
TAXIWAY 'B'  
REHABILITATION**

Name: \_\_\_\_\_

...I.P. Project Number:  
3-28-0027-038-2020

ate:	JUNE, 2020	Sheet Number:	3
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**PSP-1**



- NOTES**
1. BARRICADES AND LIGHTED PORTABLE RUNWAY CLOSURE MARKERS SHALL BE SET PRIOR TO BEGINNING WORK ON THE DAY OF THE PROJECT AND REMAIN IN PLACE UPON COMPLETION OF THE WORK ON THAT DAY.
  2. PORTIONS OF WORK FOR THIS PROJECT SHALL BE COMPLETED WITHIN THE FOLLOWING TIME FRAME: 8:00 HOURS OF 9:30 PM AND 6:00 AM LOCAL TIME. THE CURRENT SCHEDULE FOR LAST ARRIVING AND FIRST DEPARTING FLIGHTS IS: 9:30 PM (ARRIVAL) AND 6:00 AM (DEPARTURE). THE SCHEDULE FOR LAST DEPARTING WORK AFTER THE LAST DEPARTING FLIGHT OF THE DAY AND SHALL HAVE THE RUNWAY RE-OPENED IN TIME FOR THE FIRST ARRIVING FLIGHT OF THE DAY.
  3. WHEN WORK IS BEING PERFORMED WITHIN A RUNWAY OR TAXIWAY SAFETY AREA, THE RELATIVE RUNWAY OR TAXIWAY CENTERLINE SHALL BE PLACED AS SHOWN TO INDICATE THE CLOSURE.
  4. LIQUIDATED DAMAGES IN THE AMOUNT OF \$1000.00 PER MINUTE (OR PORTION THEREOF) WILL BE ASSESSED FOR FAILURE TO RE-OPEN THE RUNWAY AT THE SPECIFIED TIME EACH MORNING AFTER THE PREVIOUS NIGHT'S WORK.

- NOTES**
1. BARRICADES AND RUNWAY CLOSURE MARKERS SHALL BE SET PRIOR TO BEGINNING WORK WHICH REQUIRES CLOSURE OF THE RUNWAY, AND REMOVED PRIOR TO THE SCHEDULED TIME THE RUNWAY IS TO RE-OPEN.
  2. THE HOURS OF THE FAA AIR TRAFFIC CONTROL TOWER ARE FROM 6:00AM TO 10:00PM.



**MID-DELTA REGIONAL  
AIRPORT**  
GREENVILLE, MISSISSIPPI

**Michael Baker**  
**INTERNATIONAL**  
2113 Government Street, Suite D-3  
Ocean Springs, Mississippi  
Phone: (228) 619-2839 • [MB@BAKERINTL.COM](mailto:MB@BAKERINTL.COM)

Designer: <b>BAPER</b>	
technician: <b>KGP</b>	
Checked by: <b>JMM</b>	
Project Number: <b>177686</b>	

[illegible]

Project Name: MID-DELTA INTERNATIONAL  
AIRPORT  
TAXIWAY 'B'  
REHABILITATION

Drawing Name: **GENERAL NOTES**

A.I.P. Project Number: <b>3-28-0027-038-2020</b>	
Date: JUNE, 2020	Sheet Number: 6
Scale:	Drawing Number:

N.T.S.	GN-1

S-1. THE CONTRACTOR WILL OBTAIN, HAVE KNOWLEDGE OF, AND INCORPORATE THE FOLLOWING SAFETY PROVISIONS INTO THE CONSTRUCTION PROJECT:

- C-7. THE CONTRACTOR IS RESPONSIBLE FOR COORDINATING AND PROVIDING ALL PERMANENT AND TEMPORARY AREA.
- C-8. CONTRACTOR SHALL MAINTAIN ALL ARRIELED SAFETY DEVICES SUCH AS LIMIT LINES, FOR THE DURATION OF THE PROJECT. IF THE PROJECT REQUIRED, DAMAGED STAKES OR FLAGGING SHALL BE REPLACED IMMEDIATELY. CONTRACTOR TO SUBMIT PLAN SHOWING LOCATION OF LIMIT LINES FOR EACH PHASE AND FOR PROJECT DURATION TO THE

- C-9. BURNING OF DEBRIS WILL NOT BE ALLOWED ON AIRPORT PROPERTY.

- PROJECT SITE DURING TRAILING OPERATIONS. IF ONE WATER TRUCK IS INADEQUATE TO CONTROL DUST PROPERLY, THE CONTRACTOR SHALL FURNISH THE PROPER NUMBER OF TRUCKS OR OTHER SUITABLE MEANS TO ACCOMPLISH THIS ITEM.

- C-11. CONTRACTOR SHALL BE REQUIRED TO MOVE MEN AND EQUIPMENT OUTSIDE TAXIWAY AND RUNWAY SAFETY AREAS AS REQUIRED FOR AIRCRAFT OPERATIONS.

- C-12. ALL LIGHTS, SIGNS AND CIRCUITS ARE TO BE TESTED DAILY PRIOR TO CONTRACTOR LEAVING SITE TO ENSURE ALL ITEMS ARE DEEMED TO BE IN PROPER WORKING ORDER. ALL RUNWAY AND TAXIWAY CIRCUITS SHALL BE OPERATIONAL PRIOR TO THE CONTRACTOR LEAVING SITE EACH DAY.

- C-13. CONTRACTOR MUST COORDINATE WITH OWNER AND OTHERS INVOLVED WITH ALL CONSTRUCTION PROJECTS AT THE AIRPORT.

- C-14. CONTRACTOR IS SOLELY RESPONSIBLE FOR OBTAINING ALL PERMITS REQUIRED FOR THE PROJECT PRIOR TO ANY CONSTRUCTION ACTIVITIES.

- EXISTING CONDITIONS/UNDERGROUND AND CONCEALED FACILITIES**

### EXISTING CONDITIONS/UNDERGROUND AND CONCEALED FACILITIES

- E-1. CONTRACTOR TO FIELD VERIFY EXISTING CONDITIONS PRIOR TO BEGINNING CONSTRUCTION AND REPORT TO ENGINEER ANY VARIATIONS FROM THE INFORMATION SHOWN ON CONSTRUCTION PLANS.

- BE THE RESPONSIBILITY OF THE CONTRACTOR. CONTRACTOR SHALL REPAIR ALL UTILITIES/PIPELINES DAMAGED BY CONTRACTOR AT NO ADDITIONAL COST TO OWNER. CONTRACTOR SHALL BE ASSESSED A CABLE CUT PENALTY OF \$1,000 PER OCCURRENCE.

- E-3. FAA CABLES WILL BE LOCATED AND MARKED BY FAA PRIOR TO CONSTRUCTION. CONTRACTOR IS TO NOTIFY FAA 30 DAYS IN ADVANCE SO THAT FAA CAN SCHEDULE THE REQUIRED MARKING TO PROTECT CABLES DURING CONSTRUCTION. ANY CABLES DAMAGED DURING CONSTRUCTION WILL BE REPLACED BY CONTRACTOR. CONTRACTOR SHALL PAY ALL COSTS ASSOCIATED WITH THE REPAIR OF DAMAGED CABLES AT NO COST TO OWNER.

- ARE TO REMAIN, OR IF TO BE REMOVED, UNTIL THEY ARE DESIGNATED FOR REMOVAL.

- E-6. THE CONTRACTOR WILL BE RESPONSIBLE FOR STAKING AND GRADE CONTROL OF ALL ELEMENTS OF THE CONSTRUCTION.

- T-1. SEE SPECIFICATION SECTION 01030 AND 01530.

C-7. THE CONTRACTOR IS RESPONSIBLE FOR COORDINATING AND PROVIDING ALL PERMANENT AND TEMPORARY UTILITY CONNECTIONS TO THE STAGING AREA.

- C-8. CONTRACTOR SHALL MAINTAIN ALL AIRFIELD SAFETY DEVICES SUCH AS STAKED LIMIT LINES, FOR THE DURATION OF THE PROJECT AS

- REQUIRED. DAMAGED STAKES OR FLAGGING SHALL BE REPLACED IMMEDIATELY. CONTRACTOR TO SUBMIT PLAN SHOWING LOCATION OF LIMIT LINES FOR EACH PHASE AND FOR PROJECT DURATION TO THE ENGINEER FOR APPROVAL.

- C-9. BURNING OF DEBRIS WILL NOT BE ALLOWED ON AIRPORT PROPERTY.

- C-10. CONTRACTOR SHALL CONTROL DUST TO AN ACCEPTABLE LEVEL. THE CONTRACTOR SHALL BE REQUIRED TO KEEP A WATER TRUCK AT THE PROJECT SITE DURING HAULING OPERATIONS. IF ONE WATER TRUCK IS INADEQUATE TO CONTROL DUST PROPERLY, THE CONTRACTOR SHALL FURNISH THE PROPER NUMBER OF TRUCKS OR OTHER SUITABLE MEANS TO ACCOMPLISH THIS ITEM.

- C-11. CONTRACTOR SHALL BE REQUIRED TO MOVE MEN AND EQUIPMENT OUTSIDE TAXIWAY AND RUNWAY SAFETY AREAS AS REQUIRED FOR AIRCRAFT OPERATIONS.

- C-12. ALL LIGHTS, SIGNS AND CIRCUITS ARE TO BE TESTED "DAILY" PRIOR TO CONTRACTOR LEAVING SITE TO ENSURE ALL ITEMS ARE DEEMED TO BE IN PROPER WORKING ORDER. ALL RUNWAY AND TAXIWAY CIRCUITS SHALL BE OPERATIONAL PRIOR TO THE CONTRACTOR LEAVING SITE EACH DAY.

- C-13. CONTRACTOR MUST COORDINATE WITH OWNER AND OTHERS INVOLVED WITH ALL CONSTRUCTION PROJECTS AT THE AIRPORT.

- C-14. CONTRACTOR IS SOLELY RESPONSIBLE FOR OBTAINING ALL PERMITS REQUIRED FOR THE PROJECT PRIOR TO ANY CONSTRUCTION ACTIVITIES.

- EXISTING CONDITIONS/UNDERGROUND AND CONCEALED FACILITIES**

### EXISTING CONDITIONS/UNDERGROUND AND CONCEALED FACILITIES

- E-1. CONTRACTOR TO FIELD VERIFY EXISTING CONDITIONS PRIOR TO BEGINNING CONSTRUCTION AND REPORT TO ENGINEER ANY VARIATIONS FROM THE INFORMATION SHOWN ON CONSTRUCTION PLANS.

- E-2. CONTRACTOR SHALL BE RESPONSIBLE FOR THE LOCATION AND IDENTIFICATION OF ALL EXISTING UTILITIES AND PIPELINES IN THE CONSTRUCTION AREA. ANY EXISTING UTILITIES OR PIPELINES (ON OR OFF AIRPORT PROPERTY) DAMAGED BY CONTRACTOR SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. CONTRACTOR SHALL REPAIR ALL UTILITIES/PIPELINES DAMAGED BY CONTRACTOR AT NO ADDITIONAL COST TO OWNER. CONTRACTOR SHALL BE ASSESSED A CABLE CUT PENALTY OF \$1,000 PER OCCURRENCE.

- E-3. FMA CABLES WILL BE LOCATED AND MARKED BY FMA PRIOR TO CONSTRUCTION. CONTRACTOR IS TO NOTIFY FMA 30 DAYS IN ADVANCE SO THAT FMA CAN SCHEDULE THE REQUIRED MARKING TO PROTECT CABLES DURING CONSTRUCTION. ANY CABLES DAMAGED DURING CONSTRUCTION WILL BE REPLACED BY CONTRACTOR. CONTRACTOR SHALL PAY ALL COSTS ASSOCIATED WITH THE REPAIR OF DAMAGED CABLES AT NO COST TO OWNER.

- E-4. CONTRACTOR SHALL PROTECT ALL EXISTING LIGHTING SYSTEMS THAT ARE TO REMAIN, OR IF TO BE REMOVED, UNTIL THEY ARE DESIGNATED FOR REMOVAL.

- E-5. ANY UNPLANNED, UNAPPROVED OR ACCIDENTAL SHUTDOWN OR INTERRUPTION OF SERVICE TO ANY LIGHTING CIRCUIT OR NAVIGATIONAL AID REQUIRES IMMEDIATE NOTIFICATION OF THE AIRPORT MANAGER AND ENGINEER BY THE CONTRACTOR. ALL NECESSARY REPAIRS WILL BE MADE IMMEDIATELY AND AT CONTRACTOR'S EXPENSE.

- E-6. THE CONTRACTOR WILL BE RESPONSIBLE FOR STAKING AND GRADE CONTROL OF ALL ELEMENTS OF THE CONSTRUCTION.

**TEMPORARY MARKINGS, BARRICADES, TRAFFIC CONTROL**

- T-1. SEE SPECIFICATION SECTION 01030 AND 01530.





MID-DELTA REGIONAL  
AIRPORT  
GREENVILLE, MISSISSIPPI

Michael Baker  
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Phone: (662) 344-0000

Designer:  
KGP  
Technician:  
KGP  
Checked by:  
JIM  
Project Number:  
177 68 6

Notes:

REVISIONS		
No.	Description	Date

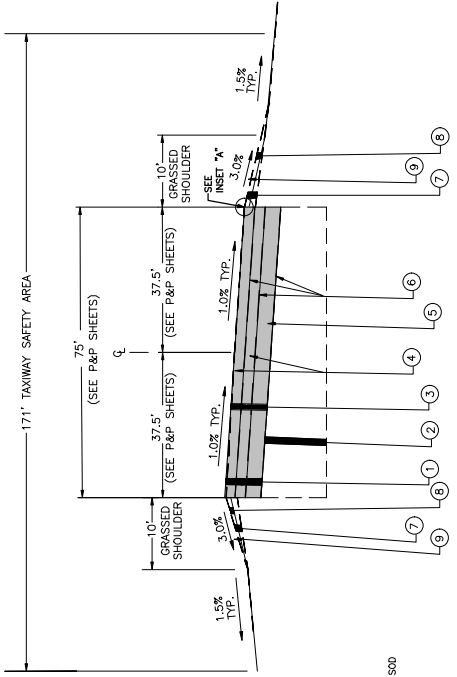
Project Name:  
MID-DELTA INTERNATIONAL  
AIRPORT  
TAXIWAY 'B'  
REHABILITATION

Drawing Name:

TYPICAL SECTIONS

ALP Project Number:  
3-28-0027-038-2020  
Date:  
JUNE 2020  
Scale:  
Sheet Number:  
9  
Drawing Number:  
N.T.S.

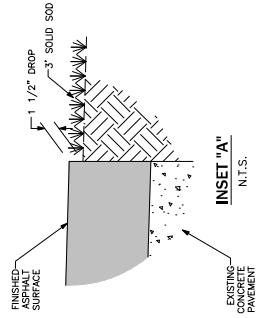
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TAXIWAY "B"  
N.T.S.  
STATION 20+25 TO 48+45.64

LEGEND

- 1 EXISTING ASPHALT SURFACE NOMINAL 7 INCH
- 2 EXISTING CONCRETE PAVEMENT, TO BE CRACKED AND SEATED, VARIABLE THICKNESS
- 3 ASPHALT MILLING
- 4 ASPHALT SURFACE COURSE, NOMINAL 2 INCH
- 5 ASPHALT SURFACE COURSE, NOMINAL 3 INCH
- 6 BITUMINOUS TACK COAT
- 7 EXISTING ASPHALT SURFACE (SHOULDER) NOMINAL 2 INCH
- 8 UNCLASSIFIED EXCAVATION
- 9 SOLID SOD



TAXIWAY "B"  
N.T.S.  
STATION 8+00 TO 20+25

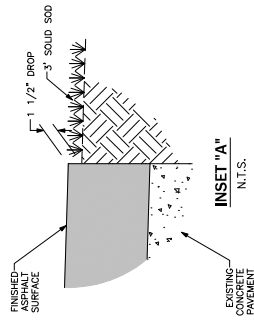
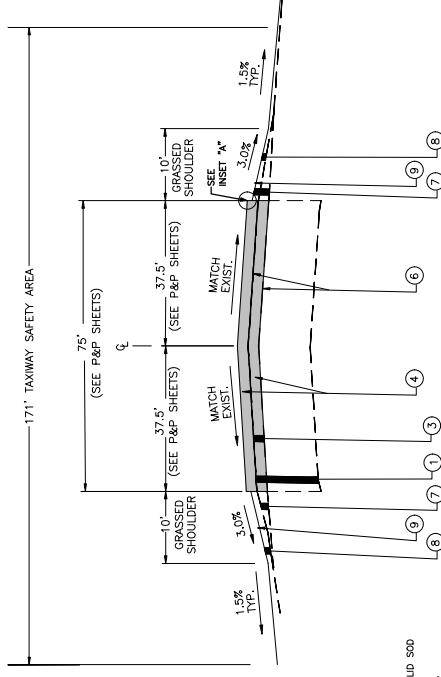
LEGEND

- 1 EXISTING ASPHALT SURFACE NOMINAL 7 INCH
- 2 EXISTING CONCRETE PAVEMENT, VARIABLE THICKNESS
- 3 ASPHALT MILLING
- 4 ASPHALT SURFACE COURSE, NOMINAL 2 INCH
- 5 ASPHALT SURFACE COURSE, NOMINAL 3 INCH
- 6 BITUMINOUS TACK COAT
- 7 EXISTING ASPHALT SURFACE (SHOULDER) NOMINAL 2 INCH
- 8 UNCLASSIFIED EXCAVATION
- 9 SOLID SOD



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Designer:	Technician:	Checked:	Project No:
KGP	KGP	JJM	1776



**TAXIWAY "A" - ADD. ALT. 1**  
N.T.S.  
STATION 0+00 - 13+39.30

- LEGEND**
- ① EXISTING ASPHALT SURFACE NOMINAL 12.5 INCH
  - ② EXISTING CONCRETE PAVEMENT, TO BE CRACKED AND SEATED, VARIABLE THICKNESS
  - ③ ASPHALT MILLING
  - ④ ASPHALT SURFACE COURSE, NOMINAL 2 INCH
  - ⑤ ASPHALT SURFACE COURSE, NOMINAL 3 INCH
  - ⑥ BITUMINOUS TACK COAT
  - ⑦ EXISTING ASPHALT SURFACE (SHOULDER) NOMINAL 2 INCH
  - ⑧ UNCLASSIFIED EXCAVATION
  - ⑨ SOLID SOD

TAXIWAY "B"	
N.T.S.	
STATION 49+95.64	- 64+83.79

- LEGEND**
- ① EXISTING ASPHALT SURFACE NOMINAL 7 INCH
  - ② EXISTING CONCRETE PAVEMENT, TO BE CRACKED AND SEATED, VARIABLE THICKNESS
  - ③ ASPHALT MILLING
  - ④ ASPHALT SURFACE COURSE, NOMINAL 2 INCH
  - ⑤ ASPHALT SURFACE COURSE, NOMINAL 3 INCH
  - ⑥ BITUMINOUS TACK COAT
  - ⑦ EXISTING ASPHALT SURFACE (SHOULDER) NOMINAL 2 INCH
  - ⑧ UNCLASSIFIED EXCAVATION
  - ⑨ SOLID SOIL

[illegible]

Project Name: MID-DELTA INTERNATIONAL AIRPORT TAXIWAY 'B' REHABILITATION

## TYPICAL SECTIONS

A.I.P. Project Number: <b>3-28-0027-038-2020</b>	
Date: <b>JUNE, 2020</b>	Sheet Number <b>10</b>
Scale:	Drawing Number

TS-2	N.T.S.
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KGP

Technician:  
KGP

Checked by:  
JJM

Project Number:  
17768

## LEGEND

PROPOSED ASPHALT PAVEMENT

REVISIONS		
No.	Description	Date By

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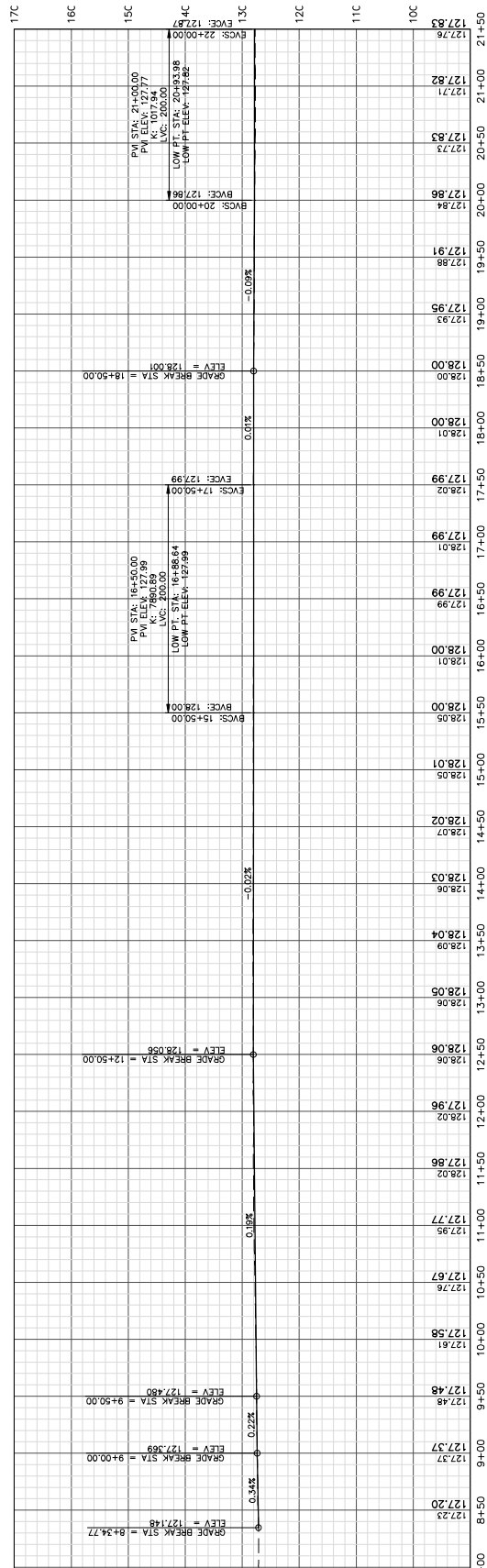
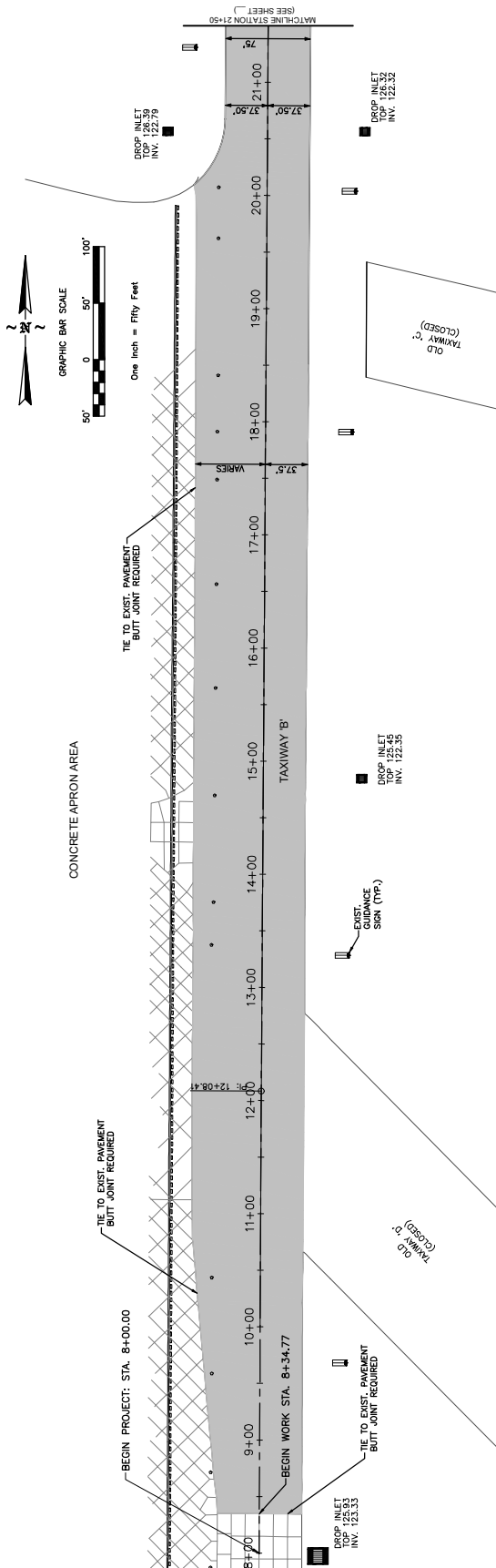
Project Name: MID-DELTA INTERNATIONAL  
AIRPORT  
TAXIWAY 'B'  
REHABILITATION

Drawing Name:

**A.I.P. Project Number:**  
**3-28-0027-038-2020**

Date:	JUNE, 2020	Sheet Number:	13
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Scale:	1" = 50'
Drawing Number:	PP-1







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Designer: KGP	Technician: KGP	Checked by: JJM	Project Number: 177686
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### LEGEND

PROPOSED ASPHALT PAVEMENT

[illegible]

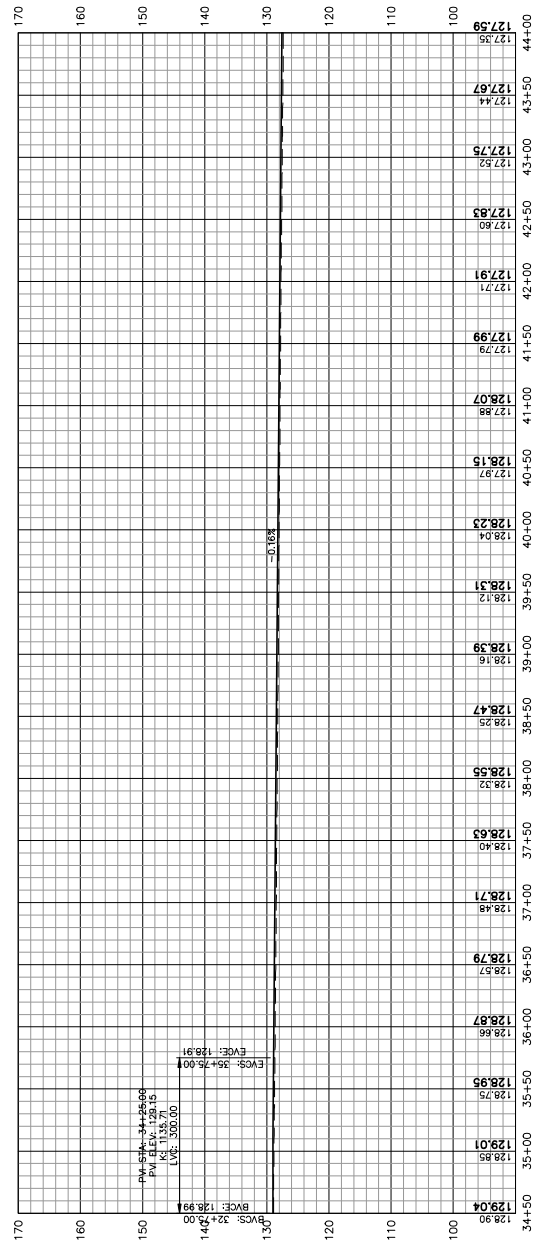
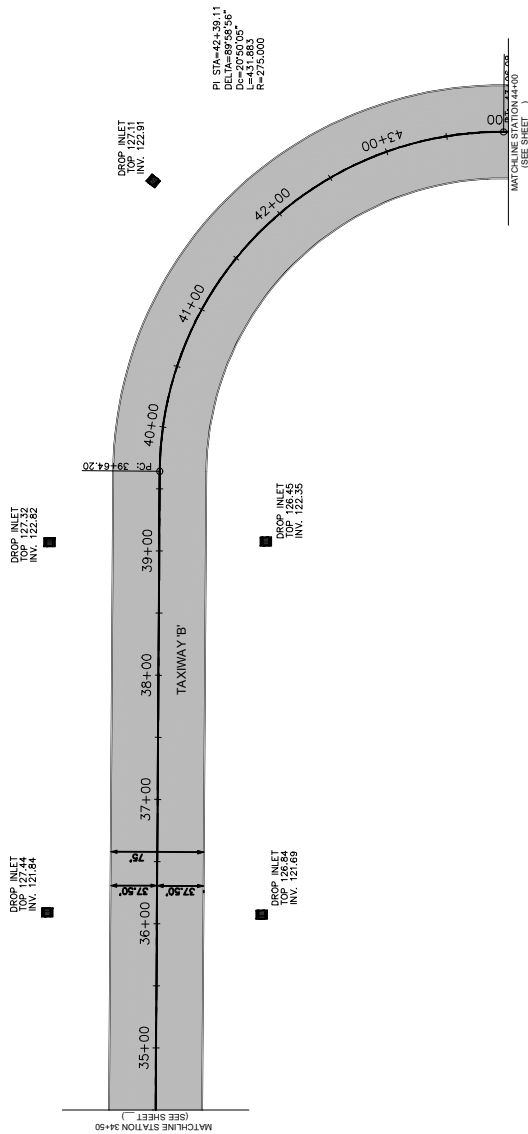
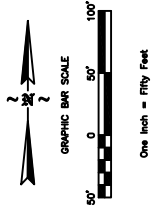
Project Name: MID-DELTA INTERNATIONAL  
AIRPORT  
TAXIWAY 'B'  
REHABILITATION

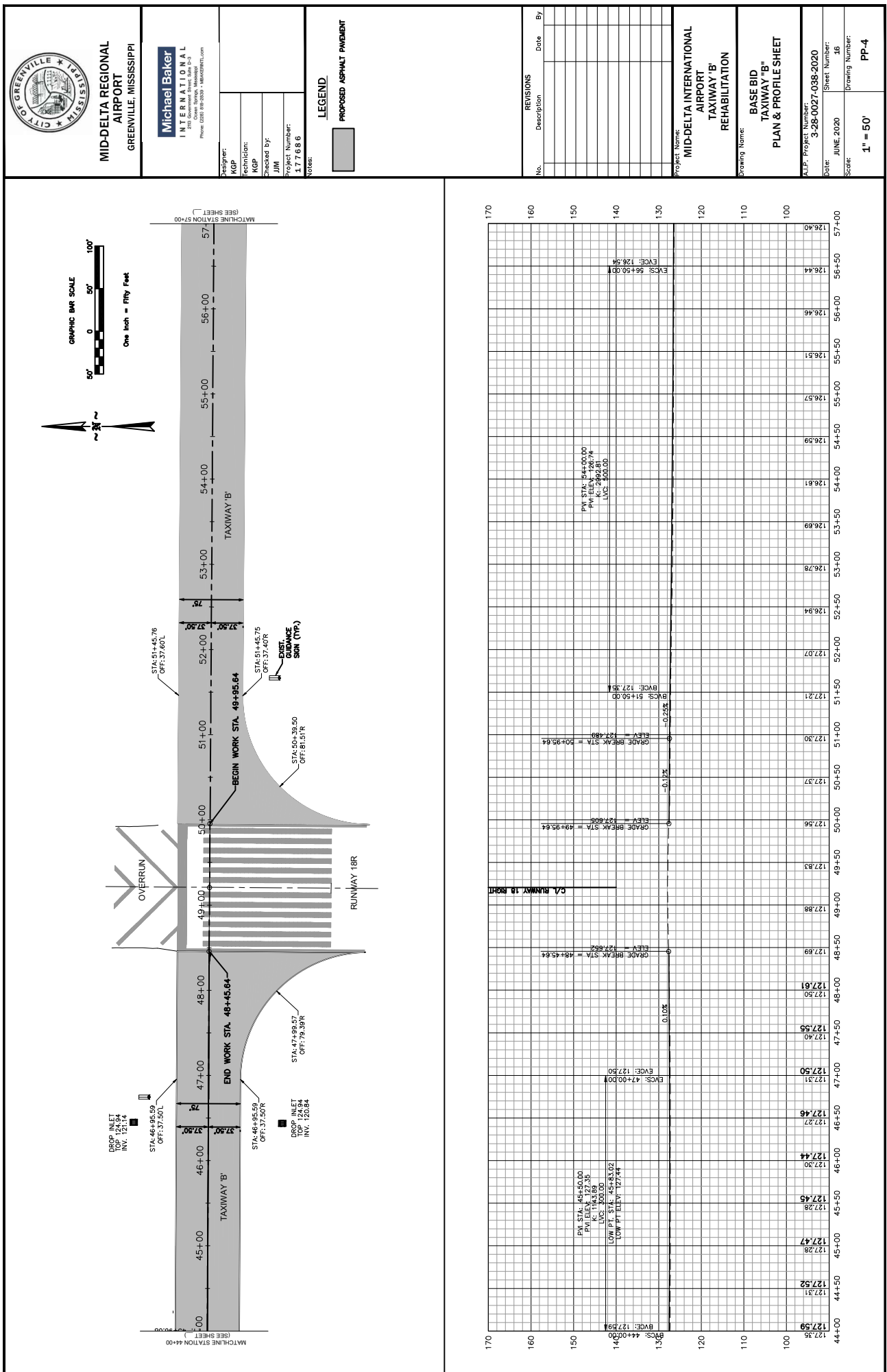
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TAXIWAY "B"  
PLAN & PROFILE SHEET**


A.I.P. Project Number:  
3-28-0027-038-2020

Date:	JUNE, 2020	Sheet Number:	15
Scale:		Drawing Number:	

1" = 50'







**MID-DELTA REGIONAL AIRPORT**  
GREENVILLE, MISSISSIPPI

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Technician: KGP  
Checked By: JIM  
Project Number: 177686  
Notes:

REVISIONS		Date	By
No.	Description		

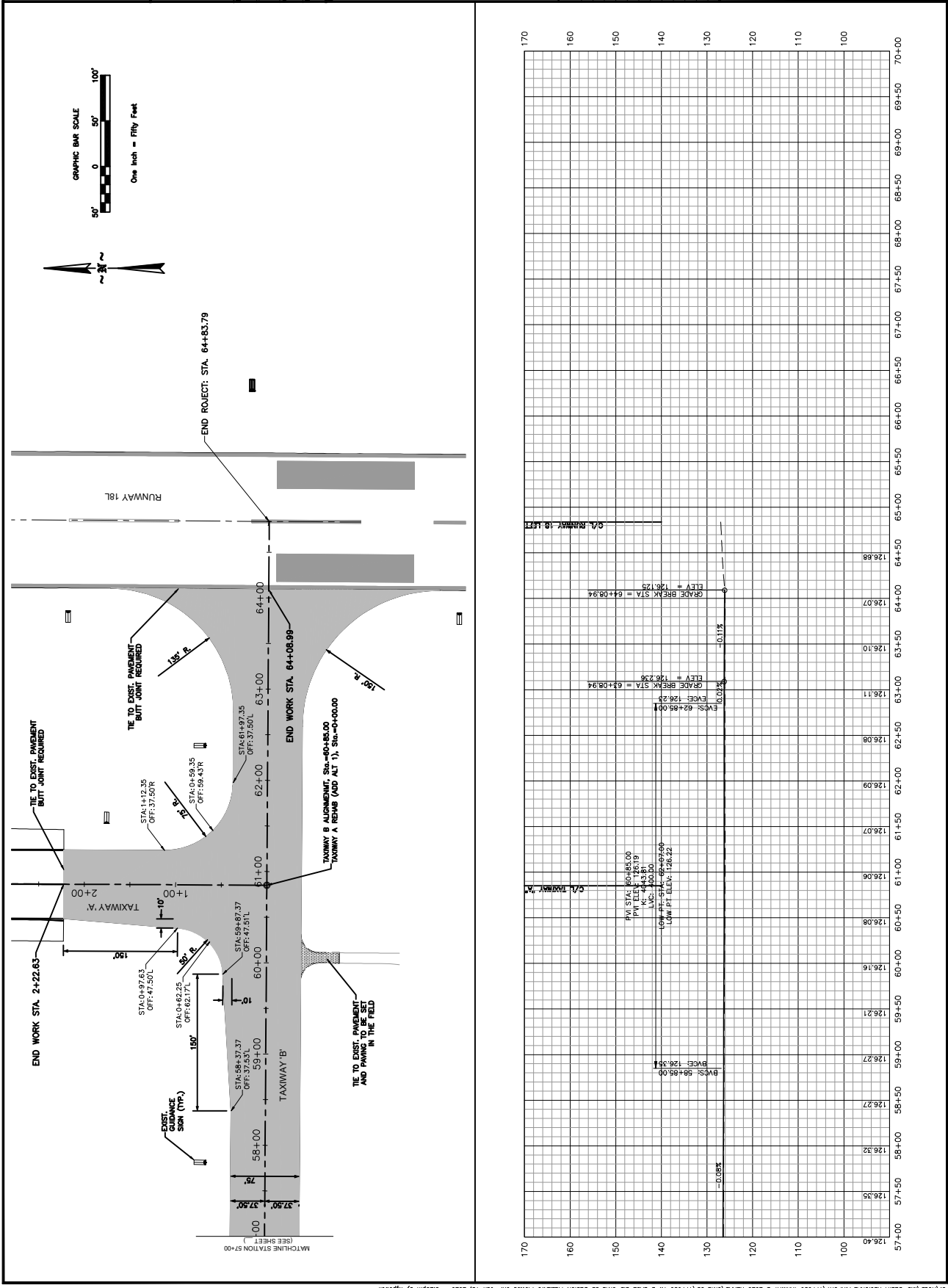
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**MID-DELTA INTERNATIONAL AIRPORT TAXIWAY 'B' REHABILITATION**

Drawing Name:  
**BASE BID TAXIWAY "B" PLAN & PROFILE SHEET**

ALP Project Number:  
**3-28-0027-038-2020**

Date: JUNE 2020  
Sheet Number: 17

Scale: 1" = 50'  
Drawing Number: PP-5



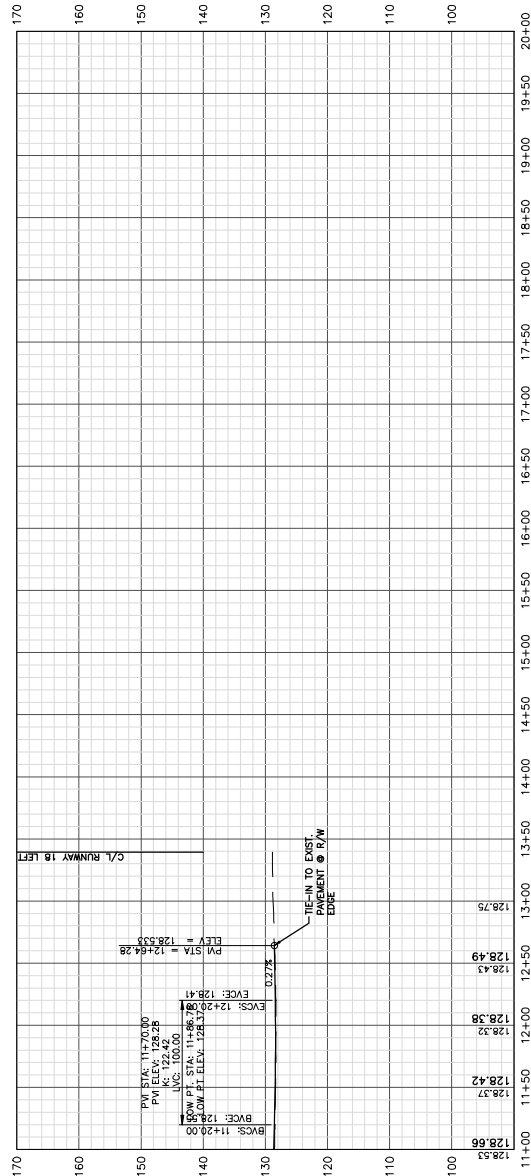
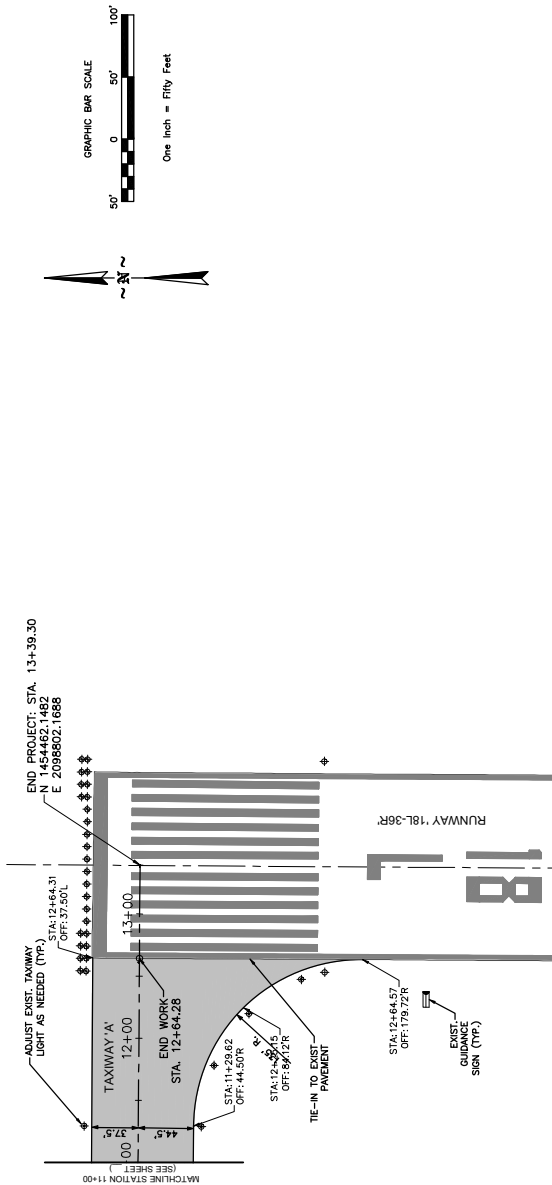




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Designer: KGP  
Technician: KGP  
Checked by: JJM  
Project Number: 17768

Notes:

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Project Name: MID-DELTA INTERNATIONAL  
AIRPORT  
TAXIWAY 'B'  
REHABILITATION

Drawing Name:  
**ADDITIVE ALTERNATE NO. 1**  
**TAXIWAY "A"**  
**PLAN & PROFILE**


A.I.P. Project Number:  
3-28-0027-038-2020

Date: \_\_\_\_\_ Sheet Number: \_\_\_\_\_

JUNE, 2020 56

Scale:	Drawing Number:
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1



**MID-DELTA REGIONAL  
AIRPORT**  
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INTERNATIONAL**  
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Designer: KGP  
Technician: KGP  
Checked By: JIM  
Project Number: 177686

**LEGEND**

EXISTING CONTOUR  
PROPOSED PAVEMENT OUTLINE  
PROJECT LIMITS  
SILT FENCE  
MULCHING  
PERMANENT SEEDING

1. PRIOR TO CONSTRUCTION, CONTRACTOR TO INSTALL ALL EROSION CONTROL MEASURES.  
2. PRIOR TO CONSTRUCTION, CONTRACTOR TO INSTALL ALL EROSION CONTROL MEASURES.  
3. PRIOR TO CONSTRUCTION, CONTRACTOR TO INSTALL ALL EROSION CONTROL MEASURES.  
4. PRIOR TO CONSTRUCTION, CONTRACTOR TO INSTALL ALL EROSION CONTROL MEASURES.

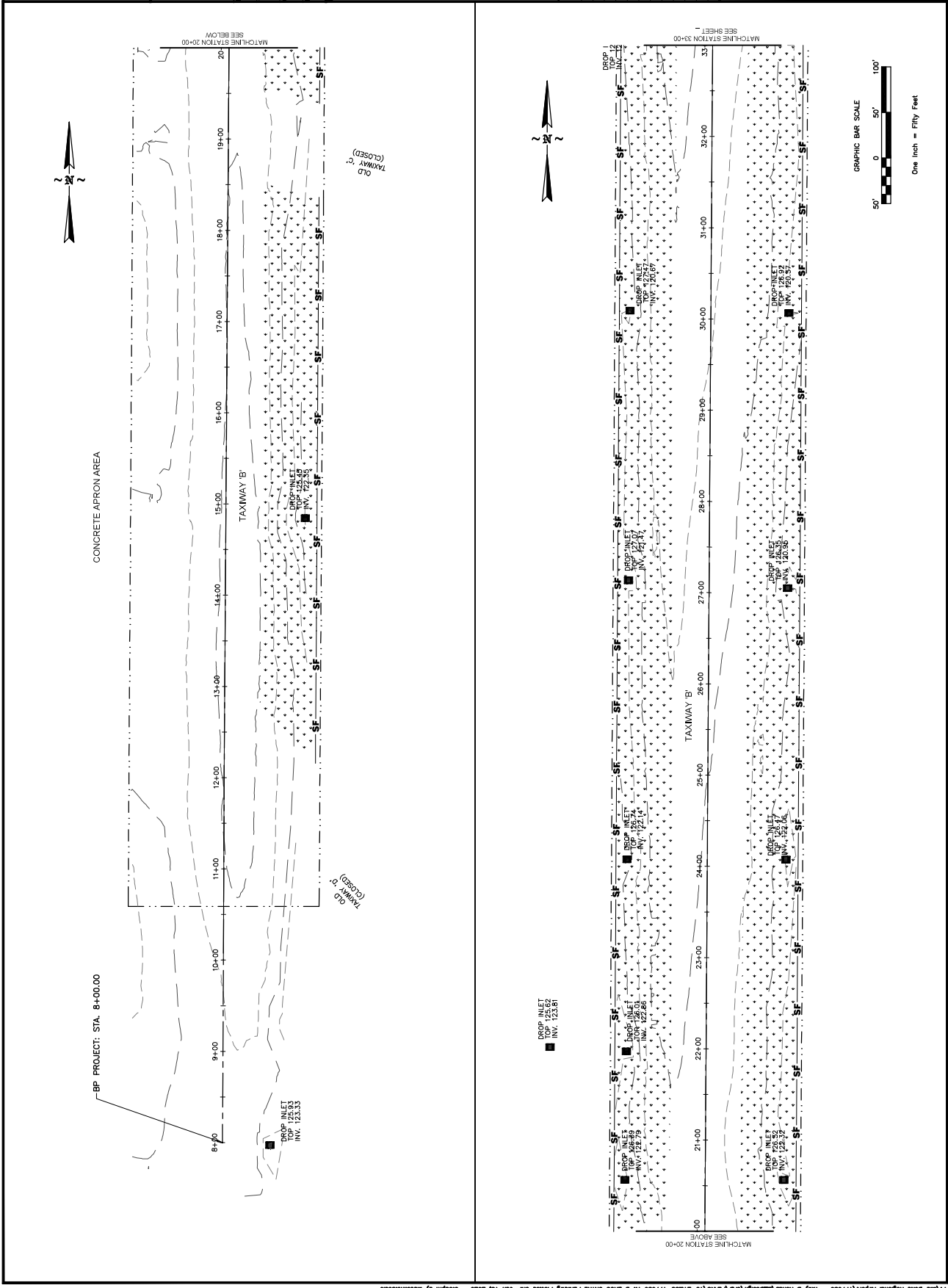
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TAXIWAY 'B'  
REHABILITATION**

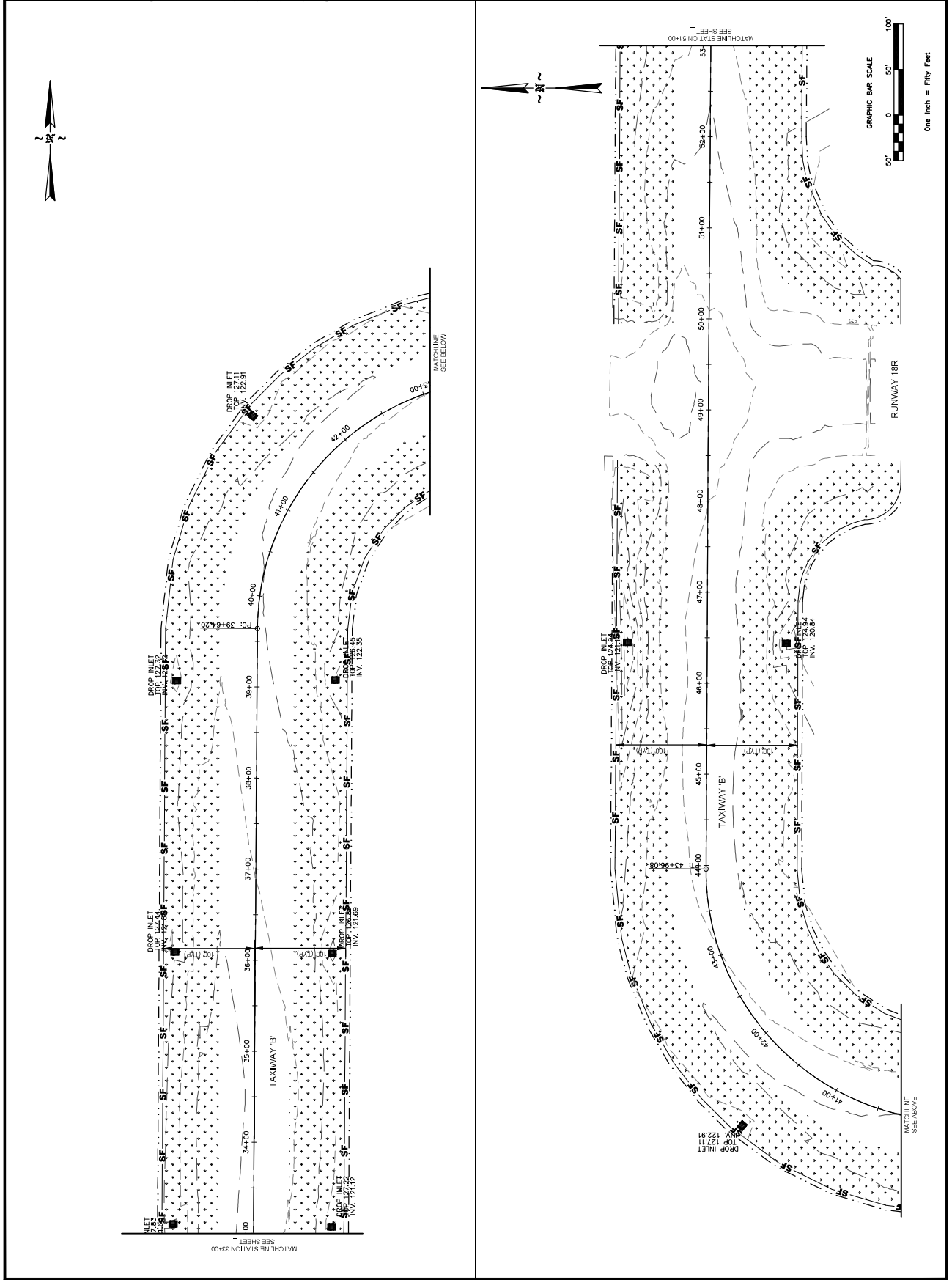
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TAXIWAY 'B'  
EROSION CONTROL PLAN**

ALP Project Number:  
3-28-0027-038-2020

Date: JUNE 2020  
Sheet Number: 19

Scale: 1" = 50'  
Drawing Number: EC-1







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Designer:	KGK
Technician:	KGK
Checked by:	JJM
Project Number:	17768

EXISTING CONTOUR

PROPOSED PAVEMENT

PROJECT LIMITS

SALT FENCE

MULCHING

PERMANENT SEEDING

1. PRIOR TO CONSTRUCTION, CONTRACTOR TO INSTALL ALL PERIMETER CONTROLS.
2. UPON COMPLETION OF GRADING ACTIVITIES, CONTRACTOR SHALL BEGIN FINAL STABILIZATION PROCEDURES AS SOON AS POSSIBLE.

REVISIONS		
No.	Description	Date By

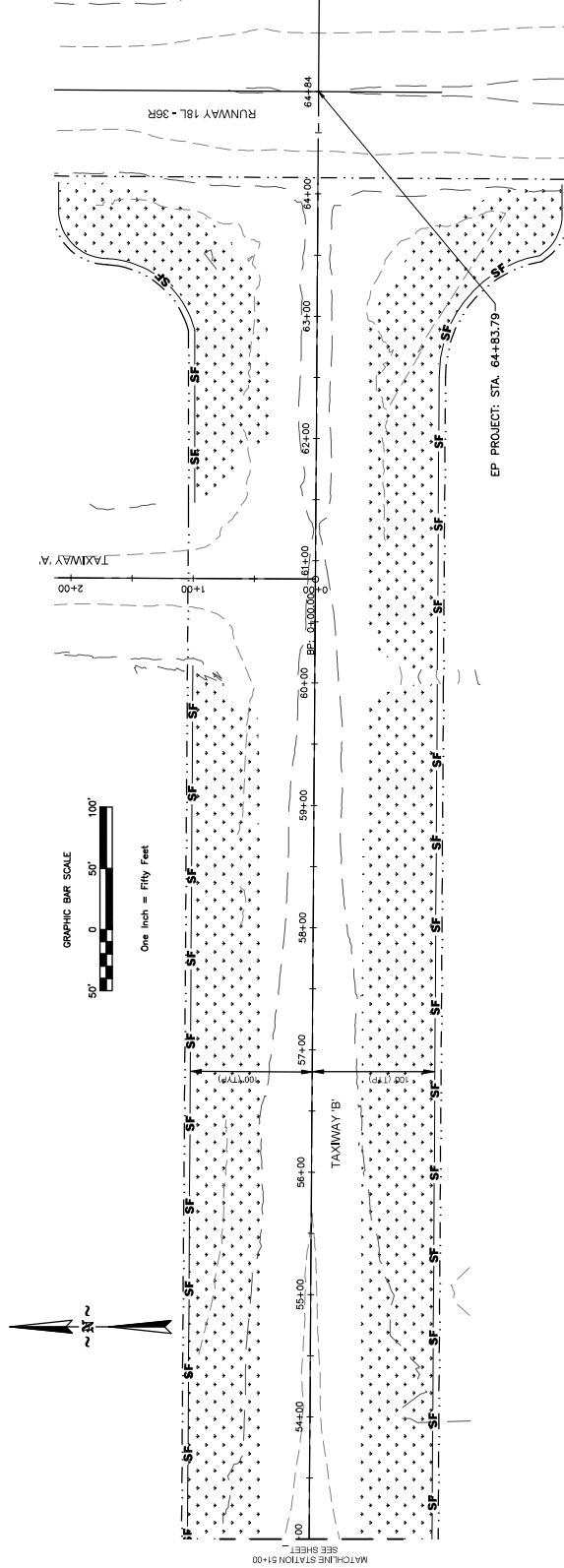
Project Name: MID-DELTA INTERNATIONAL AIRPORT TAXIWAY 'B' REHABILITATION

**BASE BID  
TAXIWAY 'B'  
EROSION CONTROL PLAN**

A.I.P. Project Number:  
3-28-0027-038-2020

Date:	JUNE, 2020	Sheet Number: 21
Scale:		Drawing Number

1" = 50'



P:\MID-Delta Regional Airport\177666 - Taxiway B Rehab\2-Design\CAD\PLANS\19-21458-177666 TW B EROS CNTRL PLAN.dwg Plotted on: Jun 18, 2020 - 6:01pm by Modeline-Seed



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Designer:  
KGP

Technician:  
KGP

Checked by:  
JJM

Project Number:  
177686

## LEGEND

EXISTING CONTOUR

PROPOSED PAVEMENT OVERLAYMENT

PROJECT LIMITS

SILT FENCE

MULCHING

PERMANENT SEEDING

1. PRIOR TO CONSTRUCTION, CONTRACTOR TO INSTALL ALL PERIMETER CONTROLS.
2. UPON COMPLETION OF GRADING ACTIVITIES, CONTRACTOR SHALL BEGIN FINAL STABILIZATION PROCEDURES AS SOON AS POSSIBLE.

REVISIONS		
No.	Description	Date By

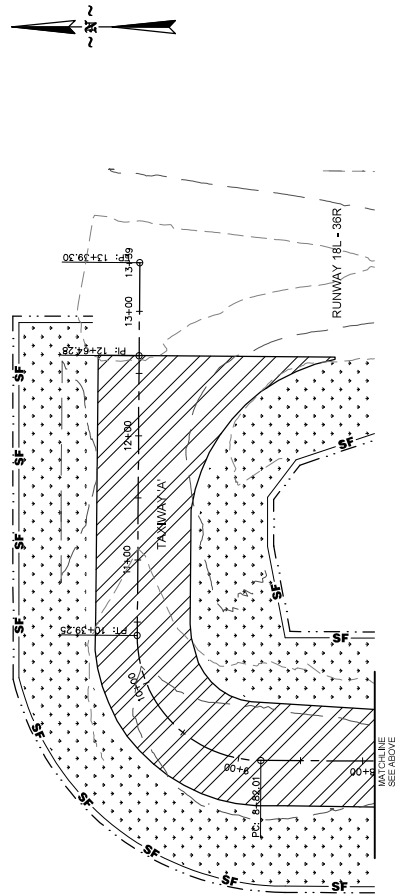
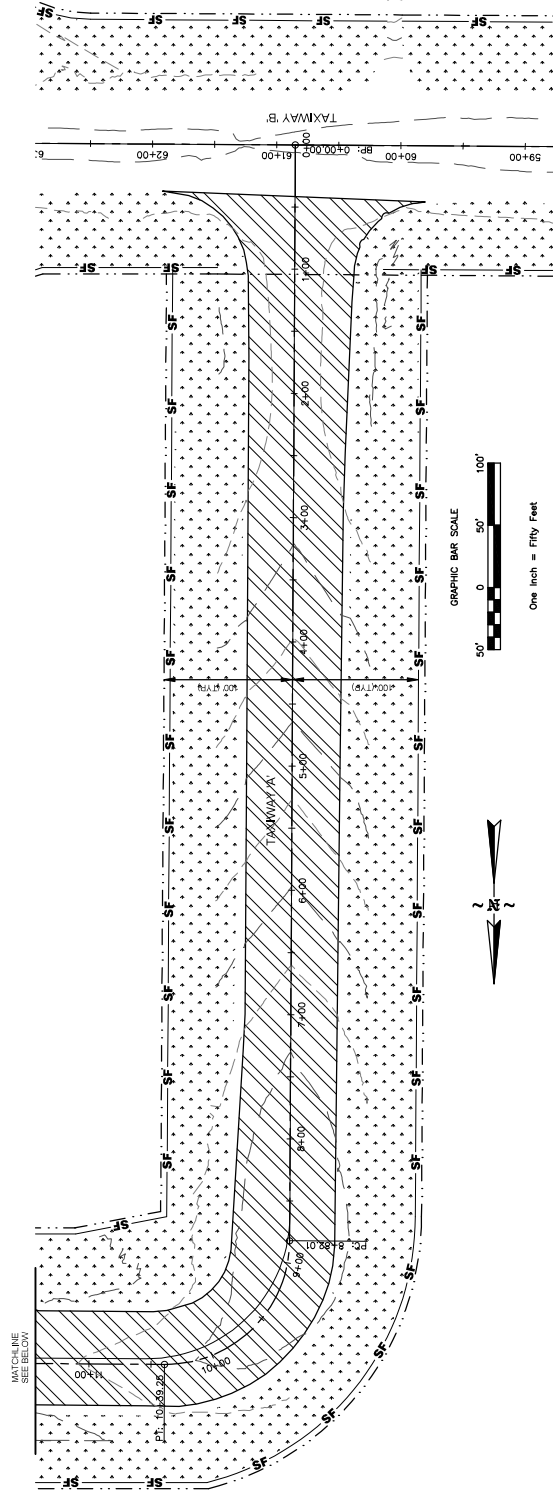
Project Name: MID-DELTA INTERNATIONAL  
AIRPORT  
TAXIWAY 'B'  
REHABILITATION

Drawing Name:

A.I.P. Project Number:  
3-28-0027-038-2020

Date:	JUNE, 2020	Sheet Number	??
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Scale:	Drawing Number
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1. APPLY TEMPORARY GRASSING TO DISTURBED AREAS WITHIN 14 DAYS OF LAND DISTURBING ACTIVITIES IF PERMANENT GRASSING IS NOT REQUIRED. APPLY TEMPORARY GRASSING TO SOIL STOCKPILES.
2. ALL AREAS RECEIVING TEMPORARY GRASS MIXTURE SHALL RECEIVE AN APPLICATION OF FERTILIZER AND BE PROTECTED WITH MULCH.
3. APPLY SEED MIXTURE AT SPECIFIED APPLICATION RATE EVENLY. DO NOT USE AN IN-AREA EXCESS OF THAT WHICH CAN BE MULCHED ON THE SAME DAY. DO NOT SOW IMMEDIATELY FOLLOWING A RAIN, WHEN GROUND IS TOO DRY, OR DURING WINDY PERIODS.
4. MULCH SHALL BE APPLIED IMMEDIATELY AFTER GRASSING. ALL MULCH SHALL RECEIVE AN APPLICATION OF TACKIFIER.
5. TACKIFIER IS TO BE APPLIED AS PER MANUFACTURERS RECOMMENDATIONS.
6. TEMPORARY MIXTURES ACCEPTABLE FOR THIS PROJECT ARE LISTED IN THE SPECIFICATIONS. SEED MIXTURES GROW RAPIDLY AND ARE LOW-MAINTENANCE.

1. APPLY PERMANENT GRASSING WHENEVER GRADING OPERATIONS ARE COMPLETE AND ALL ADDITIONAL CONSTRUCTION OPERATIONS WILL NOT IMPACT THE DISTURBED AREA. APPLY PERMANENT GRASSING TO ALL NON-CONSTRUCTION AREAS WHICH SHOW SIGNS OF EXCESSIVE EROSION.
2. ALL AREAS RECEIVING PERMANENT GRASS MIXTURE SHALL RECEIVE AN APPLICATION OF FERTILIZER AND BE PROTECTED WITH MULCH AND/OR EROSION CONTROL MATTING. APPLY MULCH AT A RATE BASED ON PH OF SOIL FOR DRY GRASSING. APPLY FERTILIZER AT A RATE BASED ON SOIL PH AND NUTRIENT REQUIREMENTS. LIGHTLY WATER TO AID THE DISSIPATION OF FERTILIZER.
3. APPLY SEED MIXTURE AT SPECIFIED RATES EVENLY IN TWO INTERSECTING DIRECTIONS BY THE USE OF A MECHANICAL SPREADER OR HYDROSEEDER. DO NOT SEED AN AREA ON DRY OR HOT SOIL. DO NOT SEED AN AREA WHEN THE AIR TEMPERATURE IS TOO DRY IMMEDIATELY FOLLOWING A RAIN, WHEN GROUND IS TOO DRY, OR DURING WINDY PERIODS.

1. HYDROSEEDING IS THE WET HYDRAULIC SPRAYING OF SEED, FERTILIZER, TACKIFIER, AND USUALLY MULCH IN A ONE-STEP OPERATION. THE SEED, FERTILIZER, TACKIFIER, AND MULCH ARE TANK TO FORM A HOMOGENEOUS SLURRY, WHICH IS THEN SPRAYED ON THE SOIL SURFACE BY A HYDRAULIC SEEDER.
2. ORDINARY MULCH IS NOT SUITABLE FOR HYDROSEEDING. MULCH FOR HYDROSEEDING IS GENERALLY VIRGIN WOOD FIBER MULCH, MANUFACTURED TO BE UNIFORMLY SUSPENDED AS A SLURRY. MULCH SHALL BE APPLIED AFTER HYDROSEEDING.

**MAINTENANCE**

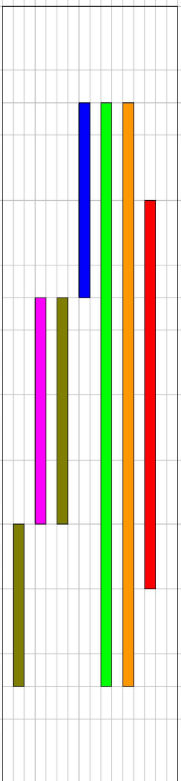
1. INSPECT FREQUENTLY WITHIN THE FIRST SIX WEEKS OF PLANTING TO SEE IF GRASS STANDS ARE UNIFORM AND DENSE, AND TO ASSURE THAT APPROPRIATE MOISTURE LEVELS ARE MAINTAINED. MAKE PROVISIONS TO WATER AS NEEDED.
2. CONTRACTOR TO REVISE AND MULCH AS NECESSARY UNTIL THE GRASS IS SATISFACTORY TO THE OWNER.
3. CHECK FOR DAMAGE CAUSED BY EQUIPMENT OR HEAVY RAINS. DAMAGED AREAS SHOULD BE REPAIRED, FERTILIZED, SEEDING, PREVENT BLOWING. TOP SOIL OR THE DOWN MULCH AS NECESSARY TO PREVENT BLOWING.

REFER TO THE PROJECT SPECIFICATION T-901 FOR ADDITIONAL INFORMATION.

### TEMPORARY GRASSING SCHEDULE

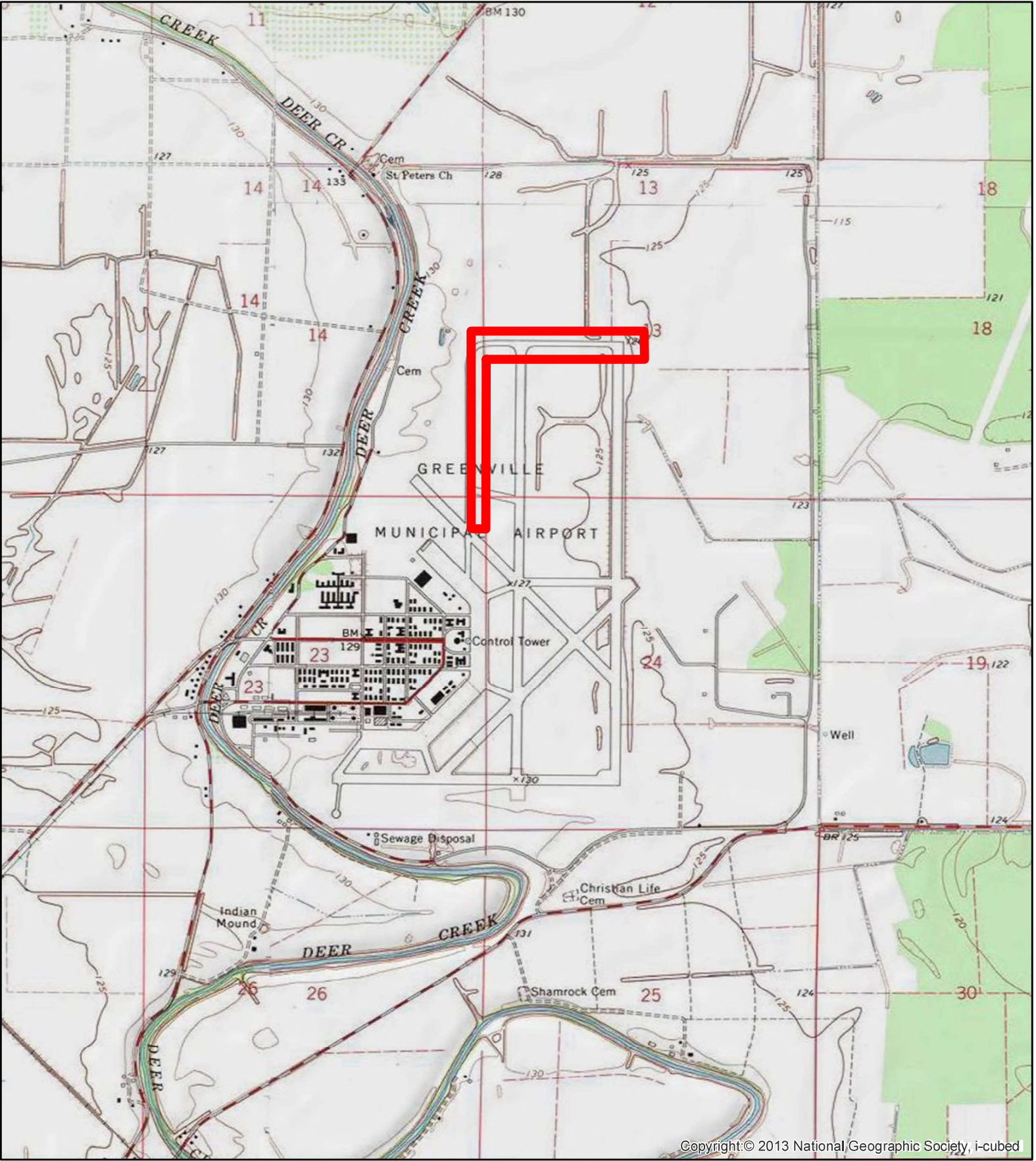


## PERMANENT GRASSING SCHEDULE



\*\*\* - TEMPORARY GRASSING ONLY FROM NOVEMBER 16TH TO FEBRUARY 15TH

**Appendix D**  
**United States Geological (USGS)**  
**Quadrangle Map**



Greenville Mid-Delta Regional Airport  
Taxiway B Rehabilitation  
Greenville, MS  
April 2020