STORM WATER POLLUTION PREVENTION PLAN (SWPPP)

AND

LARGE CONSTRUCTION NOTICE OF INTENT (LCNOI) FOR

TAXIWAY B REHABILITATION Greenville, Mississippi

Prepared for:

Mid-Delta Regional Airport Authority 166 Fifth Avenue, Suite 300 Greenville, MS 38703

June 2020

Prepared by:



Michael Baker Jr., Inc.
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Ocean Springs, MS 39564
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(228) 875-5187 fax

Rehabilitate Taxiway Site Information

The proposed construction site lies within the airfield of the Mid-Delta Regional Airport in Greenville, Mississippi. The project will disturb approximately 31 acres, to rehabilitate Taxiway B from Taxiway D north to Runway 18L. The rehabilitation will consist of removing the existing paved shoulders and replacing them with grassing. Additionally, Taxiway B will be resurfaced with asphalt pavement and new airfield markings will be installed. The majority of the site is open and is covered with grass and pavement. Soils onsite consist of silty clays and clays. Currently, water from the site drains from west to east, enters an existing drainage system, and leaves the site to the northeast. From there, the water flows a short distance through natural and man-made drainages, emptying into the Mississippi River. The nearest receiving stream is Deer Creek. Deer Creek is not on the 303(d) list for siltation, turbidity or habitat alterations; therefore, additional controls that are warranted for a site discharging to listed receiving streams are not required.

Controls

Vegetative Controls: Permanent grassing will be applied whenever grading operations are complete and additional construction operations will not impact the disturbed area. Permanent grassing will also be applied to all non-construction areas that show signs of excess erosion. Temporary grassing will be applied immediately to disturbed areas where land disturbing activities have ceased and will not resume for a period of fourteen (14) calendar days or more and permanent grass is not required. Topsoil will be stockpiled and used in areas that will be revegetated. Grass that will not compete with the grasses sown later for permanent cover shall be a quick-growing species (such as ryegrass, Italian ryegrass, or cereal grasses) suitable to the area providing a temporary cover. Grass that attracts wildlife will not be used on the project site. All areas receiving either temporary or permanent grass mixtures will receive an application of fertilizer and be protected with mulch. Mulches may be hay, straw, fiber mats, netting, bark, wood chips, or other suitable material that is reasonably clean and free of noxious weeds and deleterious materials. Mulches shall not create a wildlife attractant. Fertilizer shall be a standard commercial grade and shall conform to all federal and state regulations and to the standards of the Association of Official Agricultural Chemists. Vegetative buffer zones are undisturbed or planted vegetated areas that are between construction activities and water bodies, streets, drainage ditches, etc. The contractor will provide and maintain a 50-foot undisturbed natural buffer around waters of the United States; or provide and maintain an undisturbed natural buffer that is less than 50 feet and is supplemented by additional erosion and sediment controls, which in combination achieves the sediment load reduction equivalent to a 50-foot undisturbed natural buffer. Storm water will be directed to vegetated areas and maximize storm water infiltration to reduce pollutant discharges, unless infeasible.

Structural Controls: Perimeter controls consisting of 13,000 linear feet of silt fencing will encompass all grading operations during construction activities. Silt fence shall consist of

polymeric filaments which are formed into a stable network such that filaments retain their relative positions. Synthetic filter fabric shall contain ultraviolet ray inhibitors and stabilizers to provide a minimum of six months of expected usable construction life. Silt fence shall meet the requirements of ASTM D6461. Silt fencing will be placed well inside the right-of-way and along the edge of clearing limits to allow room for a backup fence if the first fence becomes full. The silt fences will be installed opposite erodible areas, such as newly graded fill slopes and adjacent to streams and channels. Sediment barriers consisting of straw wattle will be installed around any storm drain inlets located within and adjacent to the disturbance areas on site. In the event that wattles cannot be secured using wooden stakes, sandbags may be used to secure the wattles in place. A stabilized construction entrance will be installed prior to the intersection of any public road to help reduce the vehicular tracking of sediments on the roads. The stabilized areas will consist of a layer of aggregate to a depth of at least six (6) inches above a geotextile filter fabric for a width of at least twenty-four (24) feet and a length of at least one hundred (100) feet.

Housekeeping Practices: Trucks will be washed or rinsed prior to leaving the project site to prevent sediment transport off-site. Any detergents that may be used to wash the trucks will not be discharged into any waters. The contractor will be responsible for storing all chemicals, paints, solvents, fertilizers, pesticides, herbicides, detergents, and other potentially toxic materials in a secured approved location. The contractor will be responsible for preventing the deposition of fuels and oils within the project site at all times. Any leaks or spills identified will be immediately reported and addressed. All materials spilled, dropped, or tracked onto any public roads, including the stabilized construction entrance, will be removed daily or more frequently. Materials and equipment necessary for a petroleum spill cleanup must be kept onsite and in storage areas. Maintenance includes removal of sediment beginning when sediment accumulation reaches one-third (1/3) the capacity or height of the structure and never allowing for sediment to accumulate more than one-half (1/2) the volume or height of the ditch check structure. Adequately maintained portable sanitary facilities will be provided for construction workers. Waste receptacles and regular collection of waste will be provided.

Post-Construction/Storm Water Management Measures: At the time that satisfactory vegetation has been established, all temporary erosion control BMPs shall be removed from the site. All erosion and sediment control BMPs shall be removed by the contractor once the site is stabilized.

Implementation Sequence

Major construction activities will begin when all permits have been obtained and work is authorized. The project sequence will be as follows: 1) Install stabilized construction entrance; 2) Install perimeter control measures; 3) Protect all inlets within the disturbed areas; 4) Perform stripping and stockpile topsoil; 5) Temporarily seed stockpiles and applicable disturbed areas; 6) Complete final grading; 7) Establish permanent grassing; and 8) After the site is stabilized, remove all temporary sediment and erosion control measures.

Maintenance Plan

The project area will be inspected frequently within the first six weeks of planting to see if grass stands are uniform and dense and to assure that appropriate moisture levels are maintained. Additional vegetation will be employed where determined necessary by actual site conditions. Erosion control measures will be inspected daily and after each rain. Damaged areas will be corrected as soon as possible, but not to exceed 24 hours of the inspection unless prevented by unsafe weather conditions. Damaged areas should be repaired, fertilized, seeded, and mulched if full implementation of the approved plan does not provide for effective erosion control. Additional erosion and sediment control measures shall be implemented to control or treat the sediment source. All BMPs shall be regularly maintained. Sediment shall be removed from BMPs when the height of the sediment is one-third of the height of the BMP. The stabilized construction entrance will be maintained in a condition to allow it to perform its function to prevent offsite tracking, and the stabilized construction entrance shall be rinsed when necessary to remove accumulated mud.



Appendix A
Large Construction Notice of Intent
(LCNOI) Form



MISSISSIPPI DEPARTMENT OF ENVIRONMENTAL QUALITY (MDEQ) Large Construction Storm Water General Permit NPDES Permit MSR10

LARGE CONSTRUCTION FORMS PACKAGE

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PRIME CONTRACTOR CERTIFICATION FORM	7
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SITE INSPECTION AND CERTIFICATION FORM	12
MAJOR MODIFICATION FORM	13
REQUEST FOR TRANSFER OF PERMIT, GENERAL PERMIT COVERAC AND/OR NAME CHANGE	ЗЕ 14
INSPECTION SUSPENSION FORM	16
REQUEST FOR TERMINATION OF COVERAGE	17

These standard forms are used to apply for permit coverage under the Large Construction Storm Water General Permit and for submittals and record keeping required by permit conditions after coverage has been granted. The forms are on our website at www.deq.state.ms.us/MDEQ.nsf/page/epd_epdgeneral. Required information can be completed on screen, printed and signed.

Revised: 12/06/16



LARGE CONSTRUCTION NOTICE OF INTENT (LCNOI) FOR COVERAGE UNDER THE LARGE CONSTRUCTION STORM WATER GENERAL NPDES PERMIT

INSTRUCTIONS

The Large Construction Notice of Intent (LCNOI) is for coverage under the Large Construction General Permit for land disturbing activities of five (5) acres or greater; or for land disturbing activities, which are part of a larger common plan of development or sale that are initially less than five (5) acres but will ultimately disturb five (5) or more acres. Applicant must be the owner or operator. For construction activities, the operator is typically the prime contractor. The owner(s) of the property and the prime contractor associated with regulated construction activity on the property have joint and severable responsibility for compliance with the Large Construction Storm Water General Permit MSR10.

If the company seeking coverage is a corporation, a limited liability company, a partnership, or a business trust, attach proof of its registration with the Mississippi Secretary of State and/or its Certificate of Good Standing. This registration or Certificate of Good Standing must be dated within twelve (12) months of the date of the submittal of this coverage form. Coverage will be issued in the company name as it is registered with the Mississippi Secretary of State.

Completed LCNOIs should be filed at least thirty (30) days prior to the commencement of construction. Discharge of storm water from large construction activities without written notification of coverage is a violation of state law.

Submittals with this LCNOI must include:

- A site-specific Storm Water Pollution Prevention Plan (SWPPP) developed in accordance with ACT5 of the General Permit
- A detailed site-specific scaled drawing showing the property layout and the features outlined in ACT5 of the General Permit
- A United States Geological Survey (USGS) quadrangle map or photocopy, extending at least one-half mile beyond the facility property boundaries with the site location and outfalls outlined or highlighted. The name of the quadrangle map must be shown on all copies. Quadrangle maps can be obtained from the MDEQ, Office of Geology at 601-961-5523.

Additional submittals may include the following, if applicable:

- Appropriate Section 404 documentation from U.S. Army Corps of Engineers
- Appropriate documentation concerning future disposal of sanitary sewage and sewage collection system construction
- Appropriate documentation from the MDEQ Office of Land & Water concerning dam construction and low flow requirements
- Approval from County Utility Authority in Hancock, Harrison, Jackson, Pearl River and Stone Counties

ALL QUESTIONS MUST BE ANSWERED (Answer "NA" if the question is not applicable)

MSR10
(NUMBER TO BE ASSIGNED BY STATE)

APPLICANT IS THE:	✓ OWNER □ PRIM	ME CONTRACTOR		
	OWNER CONTACT	INFORMATION		
OWNER CONTACT PERSON:	Samuel Washington			
OWNER COMPANY LEGAL N	NAME: Greenville Mid-Del	ta Regional Airport		
OWNER STREET OR P.O. BOX	$_{ m X:}$ 166 Fifth Avenue, Suit	e 300		
OWNER CITY: Greenville	ST	ATE: Mississippi	ZIP: 38	703
OWNER CITY: Greenville OWNER PHONE #: (662)	34-3121 OWNEI	REMAIL: SWashington@	greenvillems.	org
	ME CONTRACTOR CO			
PRIME CONTRACTOR CONT				
PRIME CONTRACTOR COMP				
PRIME CONTRACTOR STREE	ET OR P.O. BOX:			
PRIME CONTRACTOR CITY:				
PRIME CONTRACTOR PHON				
	FACILITY SITE II	NFORMATION		
FACILITY SITE NAME: Mid-D				_
FACILITY SITE ADDRESS (If the indicate the beginning of the project street). 166 5th Street	ct and identify all counties the pr	oject traverses.)		
CITY: Greenville	STATE: MS	COUNTY: Washington	ZIP	38703
FACILITY SITE TRIBAL LAN	D ID (N/A If not applicable):	I/A		
LATITUDE: 33 degrees 29	minutes 0.40 seconds LON	GITUDE: 90 degrees 58 m	inutes 55.64 secon	ds
LAT & LONG DATA SOURCE	(GPS (Please GPS Project Entrance/St	art Point) or Map Interpolation):	PS (Google Ea	rth)
TOTAL ACREAGE THAT WIL	= .			
IS THIS PART OF A LARGER	COMMON PLAN OF DEVEL	OPMENT?	YES 🗆	NO ☑
IF YES, NAME OF LARGER CO AND PERMIT COVERAGE	OMMON PLAN OF DEVELO GE NUMBER: MSR10	PMENT: N/A		
ESTIMATED CONSTRUCTION			2020-07-01 YYYY-MM-DI	
ESTIMATED CONSTRUCTION	N DDA IECT END DATE.		2021-01-31	,
			YYYY-MM-DI	
DESCRIPTION OF CONSTRUC	CTION ACTIVITY: The propos	ed activity is to rehabilitate Taxiway B	rom Taxiway D north t	o Runway 18L.
PROPOSED DESCRIPTION OF The property use would not change	F PROPERTY USE AFTER Coge after construction.	ONSTRUCTION HAS BEEN	COMPLETED:	
SIC Code 4 5 8 1 N	NAICS Code 4 8 1 1 1 1			

NEAREST NAMED RECEIVING STREAM: Deer Creek		
IS RECEIVING STREAM ON MISSISSIPPI'S 303(d) LIST OF IMPAIRED WATER BODIES? (The 303(d) list of impaired waters and TMDL stream segments may be found on MDI http://www.deq.state.ms.us/MDEQ.nsf/page/TWB_Total_Maximum_Daily_Load_Section)	YES□ EQ's web site:	NO☑
HAS A TMDL BEEN ESTABLISHED FOR THE RECEIVING STREAM SEGMENT?	YES□	\mathbf{NO}
ARE THERE RECREATIONAL STREAMS, PRIVATE/PUBLIC PONDS OR LAKES WITHIN ½ MILE DOWNSTREAM OF PROJECT BOUNDRY THAT MAY BE IMPACTED B ACTIVITY?	YES□ Y THE CONST	NO ☑ RUCTION
EXISTING DATA DESCRIBING THE SOIL (for linear projects please describe in SWPPP): Nearly level, somewhat poorly drained to somewhat excessively drained silt loams and sandy loams of old natural levees: Dundee-Bosket-Beula	h-Souva	
WILL FLOCCULANTS BE USED TO TREAT TURBIDITY IN STORM WATER?	YES□	NO
IF YES, INDICATE THE TYPE OF FLOCCULANT. □ ANIONIC POLYACRYLING □ OTHER		
IF YES, DOES THE SWPPP DESCRIBE THE METHOD OF INTRODUCTION, THE LOCAT AND THE LOCATION OF WHERE FLOCCULATED MATERIAL WILL SETTLE?	ION OF INTRO YES □	DUCTION NO□

¹Acreage for subdivision development includes areas disturbed by construction of roads, utilities and drainage. Additionally, a housesite of at least 10,000 ft² per lot (entire lot, if smaller) shall be included in calculating acreage disturbed.

DOCUMENTATION OF COMPLIANCE WITH OTHER REGULATIONS/REQUIREMENTS COVERAGE UNDER THIS PERMIT WILL NOT BE GRANTED UNTIL ALL OTHER REQUIRED MDEQ PERMITS AND APPROVALS ARE SATISFACTORILY ADDRESSED

IS LO	CNOI FOR A FACILITY THAT WILL REQUIR	RE OTHER PERMITS?	YES □	NO ☑
IF Y	ES, CHECK ALL THAT APPLY: AIR	☐ HAZARDOUS WASTE	□ PRETREAT	MENT
	□ WATER STATE OPERATING □ II	NDIVIDUAL NPDES	□ OTHER: _	_
IS TH OF A	IE PROJECT REROUTING, FILLING OR CRONY KIND? (If yes, contact the U.S. Army Corps	OSSING A WATER CONVEYANCE of Engineers' Regulatory Branch fo	E YES □ or permitting requ	NO ☑ nirements.)
	HE PROJECT REQUIRES A CORPS OF ENGINUMENTATION THAT:	NEER SECTION 404 PERMIT, PRO	OVIDE APPROP	RIATE
•	The project has been approved by individual pe	ermit, or		
•	The work will be covered by a nationwide perm	it and NO NOTIFICATION to the C	Corps is required,	or
•	The work will be covered by a nationwide or get	neral permit and NOTIFICATION t	to the Corps is rec	luired
IS A (If ye	LAKE REQUIRING THE CONSTRUCTION OF s, provide appropriate approval documentation f	F A DAM BEING PROPOSED? From MDEQ Office of Land and Wat	YES □ ter, Dam Safety.)	NO 🗹
	HE PROJECT IS A SUBDIVISION OR A COMMISPOSED? Check one of the following and attack		WILL SANITA	RY SEWAGE
	Existing Municipal or Commercial System. Plea associated "Information Regarding Proposed W Hancock, Harrison, Jackson, Pearl River and Stone C of LCNOI submittal, MDEQ will accept written collection and treatment that the flows generate properly. The letter must include the estimated	/astewater Projects" form or approva Counties. If the plans and specification acknowledgement from official(s) read from the proposed project can and	al from County Ut ons can not be pro esponsible for wa	cility Authority in ovided at the time stewater
	Collection and Treatment System will be Constr permit from MDEQ or indicate the date the app	ructed. Please attach a copy of the co plication was submitted to MDEQ (D	over of the NPDES	S discharge)
	Individual Onsite Wastewater Disposal Systems of General Acceptance from the Mississippi Statengineer that the platted lots should support ind	te Department of Health or certificat	tion from a regist	opy of the Letter ered professional
	Individual Onsite Wastewater Disposal Systems feasibility of installing a central sewage collectio response from MDEQ concerning the feasibility is not feasible, then please attach a copy of the L certification from a registered professional engin disposal systems.	on and treatment system must be mad study must be attached. If a central Letter of General Acceptance from th	de by MDEQ. A I collection and w ne State Departmo	copy of the astewater system ent of Health or
INDI	CATE ANY LOCAL STORM WATER ORDINA	ANCE WITH WHICH THE PROJE	CT MUST COM	PLY:

I certify under penalty of law that this document and all attachments were prepared under my direction or supervision in accordance with a system designed to assure that qualified personnel properly gathered and evaluated the information submitted. Based on my inquiry of the person or persons who manage the system, or those persons directly responsible for gathering the information, the information submitted is, to the best of my knowledge and belief, true, accurate and complete. I am aware that there are significant penalties for submitting false information, including the possibility of fine and imprisonment for knowing violations.

Signature of Applicant¹ (owner or prime contractor)

06/26/302-0
Date Signed

Samuel Washington

Airport Director

Printed Name¹

Title

¹This application shall be signed as follows:

- For a corporation, by a responsible corporate officer.
- For a partnership, by a general partner.
- For a sole proprietorship, by the proprietor.

For a municipal, state or other public facility, by principal executive officer, mayor, or ranking elected official

Please submit the LCNOI form to:

Chief, Environmental Permits Division

MS Department of Environmental Quality, Office of Pollution Control

P.O. Box 2261

Jackson, Mississippi 39225

PRIME CONTRACTOR CERTIFICATION

LARGE CONSTRUCTION GENERAL PERMIT

Coverage No. MSR10

County

(Fill in your Certificate of Coverage Number and County)



By completing and submitting this form to MDEQ, the prime contractor is certifying that (1) they have operational control over the erosion and sediment control specifications (including the ability to make modifications to such specifications) or (2) they have day-to-day operational control of those activities at the site necessary to ensure compliance with the SWPPP and applicable permit conditions.

The owner(s) of the property and the prime contractor associated with regulated construction activity on the property have joint and severable responsibility for compliance with the permit. Notwithstanding any permit condition to the contrary, the coverage recipient and any person who causes pollution of waters of the state or places waste in a location where they are likely to cause pollution of any waters of the state shall remain responsible under applicable federal and state laws and regulations and applicable permits.

PRIME CONTRACTOR INFORMATION

PRIME CONTRACTOR CONTACT PERSON:PHONE NUMBER: ()	
PRIME CONTRACTOR COMPANY:	
PRIME CONTRACTOR STREET (P.O. BOX):	
PRIME CONTRACTOR CITY:STATE: ZIP:	···
E-MAIL ADDRESS:	
OWNER INFORMATION	
OWNER CONTACT PERSON: Samuel Washington PHONE NUMBER: (662) 334-3121	
OWNER COMPANY NAME: Greenville Mid-Delta Regional Airport	
PROJECT INFORMATION	
PROJECT NAME: Taxiway B Rehabilitation Project	
DESCRIPTION OF CONSTRUCTION ACTIVITY: The proposed project is to rehabilitate Taxiway B from Taxiway D north to Runway L. The rehabilitation will construct the proposed project is to rehabilitate Taxiway B from Taxiway D north to Runway L. The rehabilitation will construct the proposed project is to rehabilitate Taxiway B from Taxiway D north to Runway L. The rehabilitation will construct the proposed project is to rehabilitate Taxiway B from Taxiway D north to Runway L. The rehabilitation will construct the proposed project is to rehabilitate Taxiway B from Taxiway D north to Runway L. The rehabilitation will construct the proposed project is to rehabilitate Taxiway B from Taxiway D north to Runway L. The rehabilitation will construct the proposed project is to rehabilitate Taxiway B from Taxiway D north to Runway L. The rehabilitation will construct the proposed project is to rehabilitation will be proposed project in the proposed project in the proposed project is to rehabilitation will be proposed project in the	onsist
of removing the existing paved shoulders and replacing them with grassing. Additionally, Taxiway B will be resurfaced with new asphalt pavement and new airfield markings will be inst	
PHYSICAL SITE ADDRESS (If the physical address is not available indicate the nearest named road. For linear proje indicate the beginning of the project and identify all counties the project traverses.)	cts,
STREET: 166 Fifth Avenue, Suite 300	
CITY: Greenville COUNTY: Washington	
I certify that I am the prime contractor for this project and will comply with all the requirements in the above referenced general NP permit. I further certify under penalty of law that this document and all attachments were prepared under my direction or supervisi accordance with a system designed to assure that qualified personnel properly gathered and evaluated the information submitted. Be my inquiry of the person or persons who manage the system, or those persons directly responsible for gathering the information, the information submitted is, to the best of my knowledge and belief, true, accurate and complete. I am aware that there are significant penalties for submitting false information, including the possibility of fine and imprisonment for knowing violations. Ob Ob Ob Ob	on in
1 timed (value	

¹This application shall be signed as follows:

For a corporation, by a responsible corporate officer.

For a corporation, by a responsible corporate officer.

For a partnership, by a general partner.

For a sole proprietorship, by the proprietor.

For a municipal, state or other public facility, by principal executive officer, mayor, or ranking elected official.

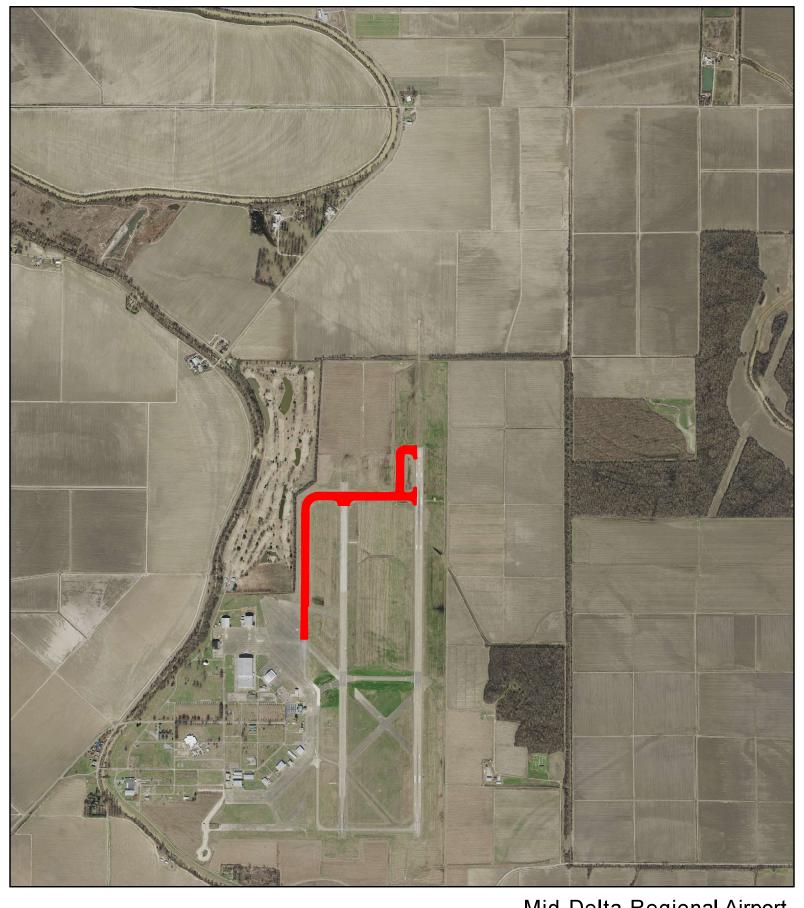
This Prime Contractors Certification form shall be submitted to:

Chief, Environmental Permits Division MS Department of Environmental Quality, Office of Pollution Control P.O. Box 2261 Jackson, Mississippi 39225

Revised: 10/25/16



Appendix B
General Location Map



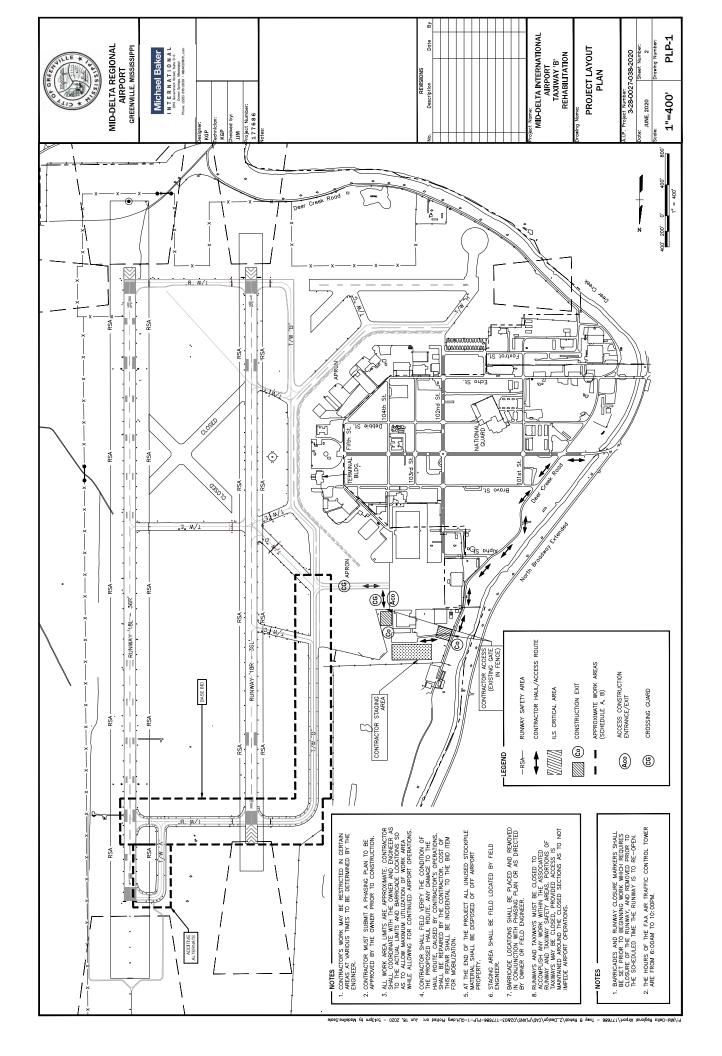


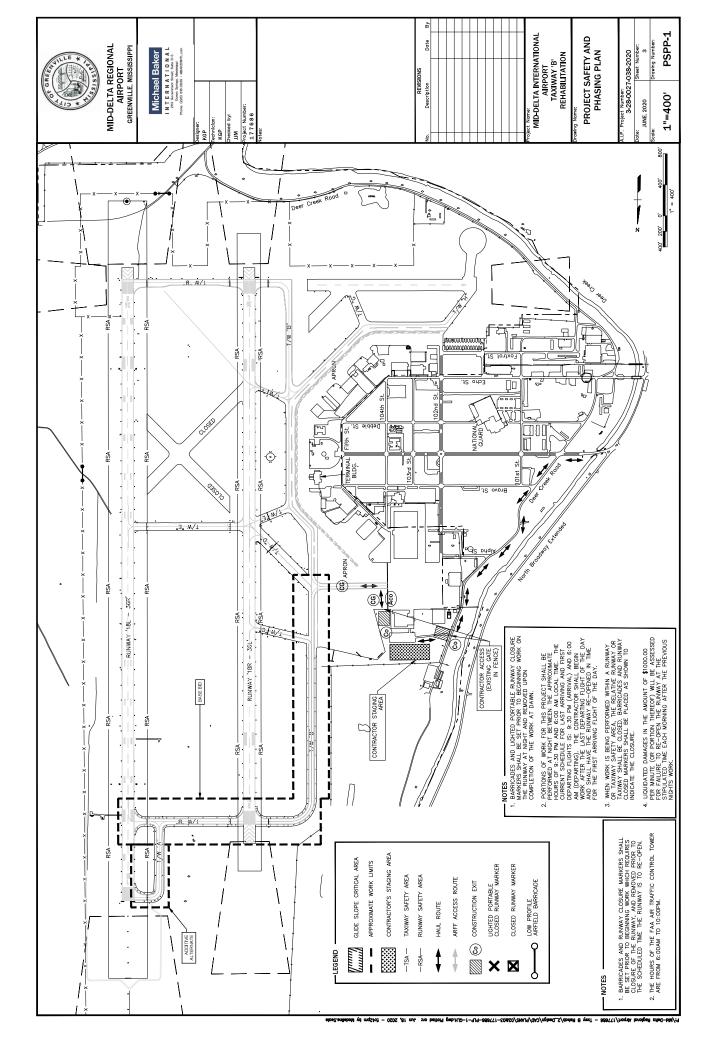
Mid-Delta Regional Airport
Taxiway B Rehabilitation
General Location Map
Greenville, MS
April 2020

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Appendix C Erosion Control Plans & Detail Sheet





GENERAL NOTES:

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PHASE I CLOSURE & PHASING:

TO MINIMIZE THE IMPACT TO AIRPORT AND FLIGHT OPERATIONS THIS PROJECT WILL BE ACCOMPLISHED IN TWO PHASES WITH AN ADDITIVE ALTERNATE.

- 1. TAXWAY B WILL BE CLOSED FROM THE INTERSECTION OF TAXWAY D (CLOSED) INTERSECTION TO THE HOLD SHORT LINE WEST OF RUNNAY IRR—508. WITH THEE I BARRICADES. THE TAXWAY BETWEEN THE HOLD SHORT TO SHORT THE TAXWAY TOWN THE MED SHORT TOWN THE THE TAXWAY TOWN THE HOLD SHORT TO THE RUNNAY SHEET AREA (FISA) AND WORK WILL NEED TO BE CHOORINATED WITH ARPORT DEPRAIDORS. OF THIS AREA SHEED ON SHEED TO SHORT TO THE RUNNAY SHEET AREA (FISA) AND WORK WILL NEED TO BE CHOORINATED WITH ARPORT DEPRAIDOR. TO THIS AREA IS ALTHORDARY COUNTY OF THE AMERICA SHEED ON THE HORTON. THE AREA IS ALTHORACIED, A TEMPORARY COUNTY WITH DATE OF THIS AREA SHEED ON THE AREA OF THIS AREA SHEED ON TH
 - CONTRACTOR WILL ENSURE ALL FOREIGN OBJECTS AND DEBRIS (FOD) REMOVED AND TAXIWAY AREA IS SWEPT AND FREE OF DEBRIS AS NEEDED.
- ONCE ALL WORK HAS BEEN ACCOMPLISHED AND APPROVED BY BOTH THE ENGINEER AND AIRPORT, ALL CLOSED AREAS SHALL BE REOPENED FOR OPERATIONS. TOTAL CONTRACT TIME ALLOWED FOR THIS PHASE SHALL BE CALENDAR DAYS.

PHASE II CLOSURE & PHASING NOTES: TO MINIMIZE THE MIPACT TO ARROPORT AND FLIGHT OPERATIONS THIS PROJECT WILL BE ACCOMPLISHED IN TWO PHASES WITH AN ADDITIVE ALTERNATE.

TO MINIMIZE THE IMPACT TO ARPORT AND FLIGHT OPERATIONS THIS PROJECT WILL BE ACCOMPLISHED IN TWO PHASES WITH AN ADDITIVE ALTERNATE. ALTERNATIVE 1 CLOSURE & PHASING NOTES:

- I. TAXIMAY B WILL BE CLOSED FROM THE HOLD SHORT LINE EAST OF RAWAY 188-208. TO THE HOLD SHORT LINE WILLS AWAY 189-208. TO THE HOLD SHORT LINE WISTON FROMWAY 181-36R WILL PARK A TRELOW X NATIONAL WILL PARK A TRELOW X NATIONAL WILL PARK A TRELOW X NATIONAL WILL BE REALDED TO BE COORDINATED WITH REPORTION OF THE TAXAWAY FROM THE HOLD SHORT TO HE COORDINATED WITH ARROPM COPIETRD ANY THE HOLD SHORT TO HE COORDINATED WITH ARROPM COPIETRD CONTRACTOR WILL BE ALLOWED INTERMITED USE OF THE AREA SHOWN SAFETY AREA (SAS) AND MORK WILL NEED LINE ON THE RAW WISTON WILL BE CALLOWED INTERMITED AT SHORT OPERATIONS. NO WORK WILL BE ALLOWED IN THE AREA WHEN CONTROL TOWNS. NO WORK WILL BE ALLOWED IN THE AREA WHEN CONTROL TOWNS IS CLOSED.

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 2. CONTRACTOR WILL WE INTRODUCE TOWN THE AREA SHOULED BY THE TOWNS AND ARROANT OPERATIONS. NO WORK WILL BE ALLOWED IN THIS AREA WHEN CONTROL TOWNS IS CLOSED.

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MID-DELTA REGIONAL AIRPORT GREENVILLE, MISSISSIPPI

INTERNATIONAL
213 Government Street Sales D-3
Cocan Spring, Mediatelpi
Phone (228) 819-2839 • WAKERINTLoom Michael Baker

MID-DEI	GREEN	ĬΝ	2	2113 Phone: (2		Designer: IXGM	Technician: Bigne	Checked by: JUM	Project Number: 177686	Notes:
1. TAXIWAY B WILL BE CLOSED FROM THE HOLD SHORT LINE EAST OF ROUNNY 18L-3SR WITH TYPE I BARRICACES. THE TAXIWAY BETWEEN THE HOLD SHORT AND RUNNWY 18R-35E AND RUNNWY 18R-35E WILL HAVE A YELLOW X IN	REMOVABLE MATERIAL PER 150/5370-26 FIGURE 2-4 TEMPORARY TAXIMAY CLOSHES, AT THE INTERSECTION OF TAXIMAY AND ENIMAY	161-308 TITE I BARKINGNES WILL BE ERECLED AT THE HOLD SHOKE LINE AND A YELLOW X IN REMOVABLE MATERIAL PER 150/5370-26 FIGURE 2-4 TEMPORARY TAXIMAY CLOSURES WILL BE INSTALLED.	2. THE PORTION OF THE TAXIWAY FROM THE HOLD SHORT TO THE RUNWAY FDGF THAT IS IN THE RINWAY SAFETY ABEA (RSA) AND WORK WILL INFED	TO BE COORDINATED WITH ARPORT OPERATIONS AND AIR TRAFFIC CONTROL TOWER.	3. IN THE AREAS EAST OF THE HOLD SHORT LINE AND RUNWAY 18L-36R, CONTRACTOR WILL BE ALLOWED INTERMITTENT ACCESS TO THESE	SECTIONS, CONTRACTOR SHALL REMOVE MEN AND EQUIPMENT IN AND OUT OF THIS AREA REQUIRED BY THE TOWER AND AIRCRAFT OPERATIONS. NO	WORK WILL BE ALLOWED IN THIS AREA WHEN CONTROL TOWER IS CLOSED. 4. CONTRACTOR WILL HAVE INDIVIDUAL WITH A UHF RADIO AND KNOWLEDGE	OF ANYONE TAND PROCEDURES POSITIONED OF RUNNAY 18R-361 TO CALL THE AIR TRAFFIC CONTROL TOWER IF VEHICLES OR EQUIPMENT NEEDS TO CROSS THIS RUNNAY.	5. CONTRACTOR WILL ENSURE ALL FOREIGN OBJECTS AND DEBRIS (FOD) IS REMOVED AND TAXIWAY AREAS ARE TO BE SWEPT AND FREE OF DEBRIS	AS NEEDED.

4 ć. 9

REGISTER OF THE STATE OF THE ST

MID-DELTA INTERNATIONAL

TAXIWAY 'B' REHABILITATION

Date

PROJECT SAFETY AND PHASING PLAN NOTES

(PHASE SPECIFIC) Project Number: 3-28-0027-038-2020 Sheet Number SP-2

NTS JUNE, 2020

GENERAL NOTES

SAFETY AND SECURITY

- THE CONTRACTOR WILL OBTAIN, HAVE KNOWLEDGE OF, AND INCORPORATE THE FOLLOWING SAFETY PROVISIONS INTO THE CONSTRUCTION PROJECT: S-1.

- CONTRACTOR SHALL MAINTAIN RADIO COMMUNICATION WITH ARPORT GROUND. CONTRACTORS SHALL HAVE WORKING CONTRACTOR. THE CONTRACTOR SHALL HAVE WORKING RADIO(S) ON-SITE AT ALL TIMES DUBING CONSTRUCTION AND SHALL ASSION ESPONSIBLE PERSONNEL TO CONTRAUDIST. MONITOR THE GROUND CONTROL FREDUDKOK (12.18 MHz). PROVIDE A RADIO AT EACH SEPARATE WORK LOCATION. OPERADAM, SAFETY ON AIRPORTS DURING CONSTRUCTION – AC AIRPORT SAFETY SELF-INSPECTION – AC 150/5200-18C. PAURINE, MARCHIE, AND LUGHING OF VEHICLES USED ON AIRPORT – AC 150/5210-50.

S-2.

- NOTICE TO AIRMEN (NOTAMS) THE CONTRACTOR SHALL SCHEDULE AND PROVIDE THE NECESSARY INCOMRATION ON CONSTRUCTION CONDITIONS SO THAT OWNER CAN ADJUST NOTAM MANAGER AND ISSUE NOTAM(S) IN ACCORDANCE WITH ESTABLISHED CRITERIA. 5-3.
 - ALL CONSTRUCTION TRAFFIC SHALL ENTER AND EXIT THE PROJECT AREA THROUGH THE PROJECT ACCES GATES SHOWN ON THESE PLANS. CONTRACTOR WILL BE RESPONSIBLE FOR SECURITY OF ALL CATES IN ACCORDANCE WITH THE AIRPORT'S APPROVED SECURITY PROGRAM.
 - AREAS OUTSIDE THE PROJECT LIMITS ARE DESIGNATED AS RESTRICTED AREAS. THE CONTRACTOR'S FORCES ARE PROHIBITED FROM ENTERING RESTRICTED AREAS AT ANY TIME, UNLESS SPECIFICALLY AUTHORIZED BY THE ENGINEE OR ARRORT OWNER.
- ALL VEHICLES USED ON THE AIRFIELD SHALL MEET AIRPORT REQUIREMENTS FOR MARKING AND LIGHTING. S-6.
- FOR ADDITIONAL SAFETY AND SECURITY REQUIREMENTS, SEE SPECIFICATIONS. (SECTION 01030) S-7.

ROUTES, STACING AREAS AND CONSTRUCTION ACTIVITIES

- THE CONTRACTOR SHALL CONDUCT HIS CONSTRUCTION OPERATIONS AS SHOWN ON THE PROJECT LAVIOUT AND SETET PARA NAD HIS SHOWN ON THE PROJECT LAVIOUT AND SETET PARA NAD HIS CONSTRUCTION ACTIVITIES THAT HE ENGINEER TO MINIMIZE DISRUPTION TO ARRORD OPERATIONS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO ARRORD OPERATIONS. IT IS THE CONTRACTOR'S AND TO MINIMIZE INTERFERENCE TO ARRORAT OPERATIONS DURING CONSTRUCTION. C-1.
- THE CONTRACTOR'S STAGING AREA AND HAUL ROUTES SHOWN ON THE PLANS ARE GENERAL AND TO PEN INFORMATION PHEROSES ONLY. THE ARAPROVED STADING AFEAS AND HAUL ROUTES WILL BE APPROVED BY OWNER PRIOR TO CONSTRUCTION.
- ALL EXISTING GRASSED AREAS WHICH ARE DISTURBED AS PART OF THE CONTRACTOR'S ACCESS ROAD, CONTRACTOR'S STAGING AREA, AND HALL ROUTES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION AFTER COMPLETION OF THE PROCEET. IN ADDION, ALL EXISTING ROAD, ARRONS AND TAXIMANS THAT WILL BE USED AS THE CONTRACTOR'S HALL BE MANTANED DIJRING CONSTRUCTION AND RESTORED TO THEIR PRE—CONSTRUCTION CONDITION. NO SEPARATE MEASUREMENT OR PANKIN WILL BE MADE FOR THIS TIEM.
- EXCEPT WHERE DESIGNATED ON PLANS OR AS AUTHORIZED BY ENGINEER, CONTRACTOR WILL NOT BE ALLOWED TO USE ANY OF THE EXISTING ENVANCES OR TAXMAYS AS PART OF THE HAUL ROAD.
- ACTIVE TAXIWAYS, RUNWAYS, SHALL BE KEPT FREE OF ALL DEBRIS, DIRT, EIGH, AT ALL TIMES. AMY SEINLAGE OF SOIL, DEBRIS OR OTHER WAITERALS SHALL BE CLEANED UP INMEDIATELY USING SHEPOPRATE POWER SMEEPERS, BLOWRES OR OTHER APPROVED EQUIPMENT, BY THE CONTRACTOR AFTER ROMPT NOTIFICATION BY THE CONTRACTOR AFTER PROMPT NOTIFICATION BY THE CONTRACTOR AFTER PROMPT NOTIFICATION BY THE MAINTERS SHALL BE ADSCILAFED. YOUR POLICE OF ALL TIMES. A ROMPT NOTIFICATION SHALL BE MAILEMENTED BY THE CONTRACTOR, AND COORDINATED WITH THE AIRPORT OWNER, AND THE DISNEER.
- ALL EQUIPMENT MUST BE RETURNED TO THE STAGING AREA AT THE END OF EACH WORK NOX AND WHEN NOT ENGEGED IN THE CONSTRUCTION DURING NON-WORKING DAYS AND NIGHTS. OWNER WILL DESIGNATE AREAS FOR CONTRACTOR'S EMPLOYEES AUTO PARKING. Regional Airport/177686 - Txwy B Rehab/2_Design/CAD/PLANS/06-177686 Gen NOTES.dwg Plotted on:

- THE CONTRACTOR IS RESPONSIBLE FOR COORDINATING AND PROVIDING ALL PERMANENT AND TEMPORARY UTILITY CONNECTIONS TO THE STAGING AREA.
- CONTRACTOR SHALL MAINTAIN ALL AIRFIELD SAFETY DEVICES SUCH AS STAKED UMIL LINES, FOR THE DURATION OF THE PROJECT AS REQUIRED. DAMAGED STAKES OR FLAGGING SHALL BE REPLACED IMMEDIATELY. CONTRACTOR TO SUBMIT PLAN SHOWING LOCATION OF LIMIT LINES FOR EACH PHASE AND FOR PROJECT DURATION TO THE ENGINEER FOR APPROVAL. 0-8
- BURNING OF DEBRIS WILL NOT BE ALLOWED ON AIRPORT PROPERTY.
- CONTRACTOR SHALL ENE REQUIRED TO RN ACCEPTABLE LEVEL. THE CONTRACTOR SHALL BE REQUIRED TO KREP A WAITER TRUCK AT THE PROJECT SITE DURING HAULING OPERATIONS. IF ONE WAITER TRUCK IS INAGEQUATE TO CONTROL DUST PROPERLY. THE CONTRACTOR SHALL TURNISH THE PROPER NUMBER OF TRUCKS OR THERE SUITABLE MEANS TO ACCOMPLISH THEN TRUCK. C-10.
- CONTRACTOR SHALL BE REQUIRED TO MOVE MEN AND EQUIPMENT OUTSIDE TAXINAY AND RUNMAY SAFETY AREAS AS REQUIRED FOR AIRCRAFT TREARINDS.
- ALL LIGHTS, SIGNS AND CIRCUITS ARE TO BE TESTED "DAILY" PRIOR TO CONTRACTOR LEANING SITE TO ENSURE ALL ITEMS ARE DEEMED TO BE IN PROPER WORKING OPERS. ALL LIWINMAY AND TAXIMAY CIRCUITS SHALL BE OFERATIONAL PRIOR TO THE CONTRACTOR LEANING SITE EACH DAY. C-12.
- CONTRACTOR MUST COORDINATE WITH OWNER AND OTHERS INVOLVED WITH ALL CONSTRUCTION PROJECTS AT THE AIRPORT. C-13.
- CONTRACTOR IS SOLELY RESPONSIBLE FOR OBTAINING ALL PERMITS REQUIRED FOR THE PROJECT PRIOR TO ANY CONSTRUCTION ACTIVITIES.

EXISTING CONDITIONS/UNDERGROUND AND CONCEALED FACILITIES

- CONTRACTOR TO FIELD VERIEY EXISTING CONDITIONS PRIOR TO BEGINNING CONSTRUCTION AND REPORT TO ENGINEER ANY VARIATIONS FROM THE INFORMATION SHOWN ON CONSTRUCTION PLANS. E-1.
- CONTRACTOR SHALL BE RESPONSIBLE FOR THE LOCATION AND IDENTIFICATION OF ALL XENSING UNITIES AND PRELLIKES IN THE CONSTRUCTION AREA. ANY ENSITING UTILITIES OR PRELLIKES IN ON FOR PROPERTY DAMAGED BY CONTRACTOR SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. CONTRACTOR SHALL REPARK ALL UTILITIES/PIPELIMES DAMAGED BY CONTRACTOR SHALL REPARK ALL UTILITIES/PIPELIMES DAMAGED TO CONTRACTOR SHALL BE ASSESSED A AGBIEROU. PENALTY OF \$1,000 PER OCCURRENCE. E-2.
- FAR CABLES WILL BE LOCATED AND MARKED BY FAX PROPER TO CONSTRUCTION. CONFINCTION IS TO NOTIFY FAX, 30 DAYS IN ADMACE SO THAT FAX CAN SCHEDULE. THE REQUIRED MARKING TO PROTECT CONSTRUCTION. ANY CABLES DAMACED DURING CONSTRUCTION WILL BE REPLACED BY CONTINGATORS. CONTRACTORS SASOCIATED WITH THE REPAIR OF DAMACED CABLES AT NO COST TO OWNER. E-3
- CONTRACTOR SHALL PROTECT ALL EXISTING LIGHTING SYSTEMS THAT ARE TO FRAMAN, ON IF TO BE REMOVED, UNTIL THEY ARE SESIONATED FOR REMOVAL. E-4.
- WAY UPLANNED, UNAPPROVID OR ACCIDENTA SHITDOWN OR NITERHETON OF SERVICE TO ANY LIGHTING CIRCLIT OR NIVOATIONAL AD REQUIRES IMMEDIATE NOTIFICATION OF THE MARPORT MANGERS AND ENGINEER BY THE CONTRACTORS. ALL NECESSARY REPAIRS MILL BE MADE IMMEDIATELY AND AT CONTRACTORS E. PERSISE. E-5
- THE CONTRACTOR WILL BE RESPONSIBLE FOR STAKING AND GRADE CONTROL OF ALL ELEMENTS OF THE CONSTRUCTION.

TEMPORARY MARKINGS, BARRICADES, TRAFFIC CONTROL

T-1. SEE SPECIFICATION SECTION 01030 AND 01530.



CONCRETE WASHDOWN: WORKELE DRUMS AT THE CONSTRUCTION SITE IS PROHIBITED. IF THE CONTRACTOR WOSHDOWN OF TOOLS, CONCRETE MACE CHUTES AND/OR HOPPERS, THE CONTRACTOR SHALL SUBMIT A CONCRETE WASHDOWN AREA PLAN TO THE OWNER AND ENDIRECT FOR APPROVAL.

PETROLEUM SPILLS AND LEAKS:

AN SPCC PLAN WILL BE DEVELOPED AND APPROVED PRIOR TO INSTALLATION OF FUEL TANKS. THIS PLAN WILL REQUIRE THE FOLLOWING:

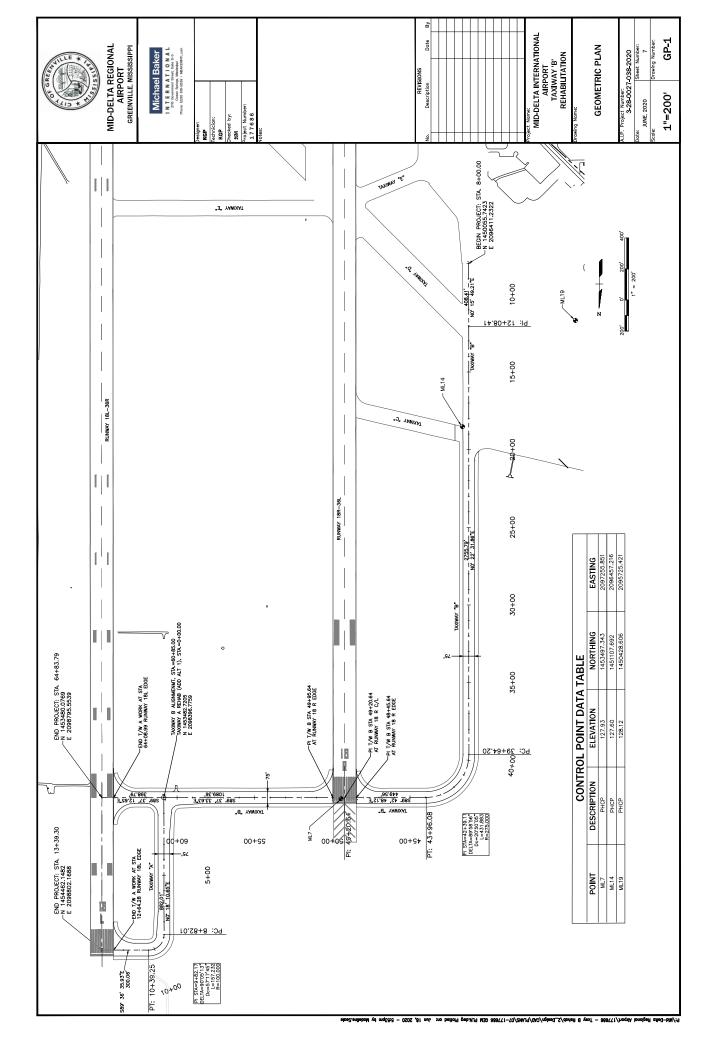
1.1ALL BREE SOIL, AROUND FIELING, AREA, LINED TO PREVENT SEEPAGE INTO SOIL. IN ADDITION, A 2.TERTIAN WILL BE PROVIDED IN THE MOBILE TRUCK AREA. 3.SECONDARY CONTAINMENT WILL BE PROVIDED IN THE FORM OF A CONTAINMENT PAD WITH A WALE SIZED (ONLY AREA PROVIDE DIN THE FORM OF A CONTAINMENT PAD WITH A WALE SIZED (ONLY ARPROVED EQUAL) FOR THE APPROPRIATE REFULLING TRUCK.

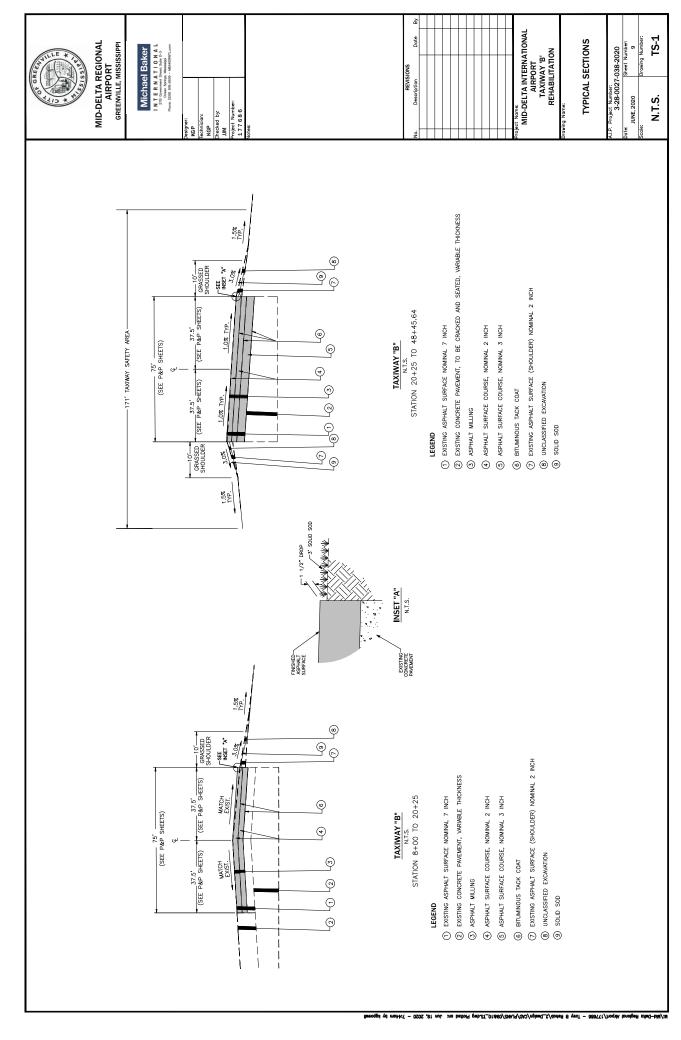
THE CONTRACTOR SHALL PREPARE AN SPCC PLAN FOR FUEL STORAGE AREAS DURING CONSTRUCTION.

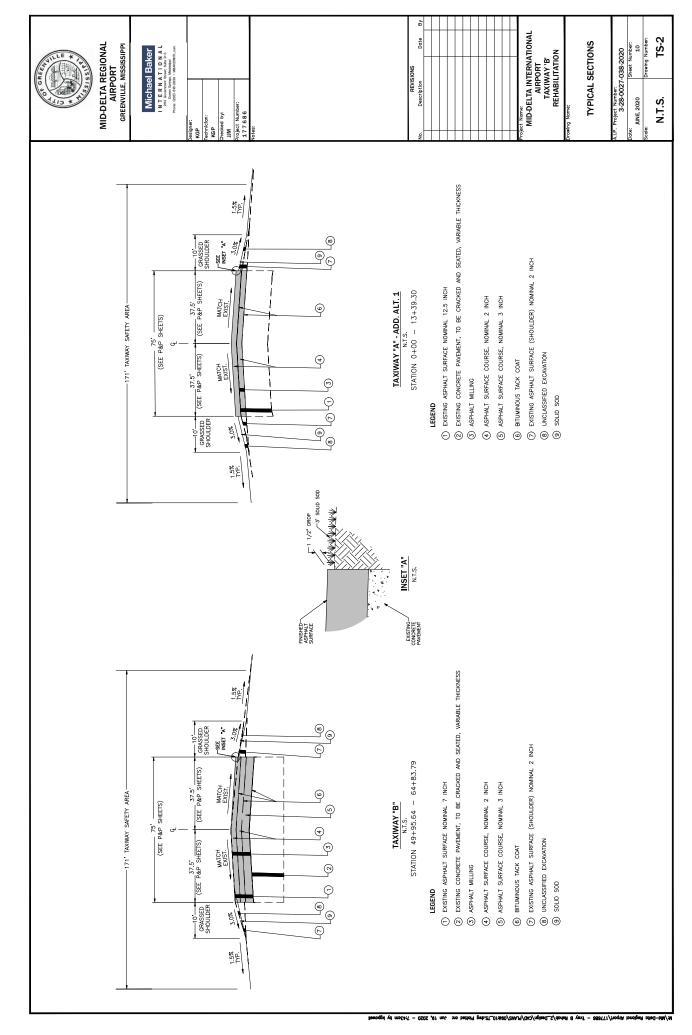
MATERIALS AND EQUIPMENT NECESSARY FOR PETROLEUM SPILL CLEANUP MUST BE KEPT ONSITE NON STORMERS. THESE WAY NOLDED BEROOKS, DESTROYS, MORES, RASS, GLONES, COROMERS, STORMERS, SHELF, BEROOKS, SHOW NOLDED BEROOKS, DESTROYS, MORES, SHOW THE PROPERTY OF THE REPORTED AND ADUSTED SA NECESSARY TO PREVENT FULLY REPORTED. ALL SPILLS MUST BE CHANTED BENEAUTHOR. THE DESTROYS SHOW THE PERFORMED UPPLANTED AND ADUSTED SA NECESSARY TO PREVENT FULLY REPORTED. AS REQUIRED WITHOUT STAFFACE WAIRTS, THE MUST SHOW THE MAPPET SURFACE WAIRTS HERE DAYS. AT I BROOKED, TO STAFFACE WITHOUT SHOW THE USENDED PROPERSONAL WITHOUT SHOW THE WAIRTS SHOW THE RELAXION OF SURFACE WAIRTS MAPPED. THE SPILL SHOW THE SHOW THE LESS THAN 25 GLALONS AND NO SURFACE WAIRTS MHAPETS. THE SHUL WILL BE CLEANED UP AND LOCAL AGENCIES MUST BE CONTACTED AS REQUIRED. THE CONTRACTED SHALL NOTIFY THE LICENSED PROFESSIONAL WITH INCLUDES CAPACITY EQUIPMENT) OF I ANY ONE PIECE OF EQUIPMENT HAS A CAPACITY OF ANY ONE PIECE OF EQUIPMENT HAS A CAPACITY AND COUNTINEMENT AND COUNTINEMENT HAS A SPILL SHEEVERHON CONTAINMENT AND COUNTINEMESURES DAY HAIL LICENSED PROFESSIONAL.

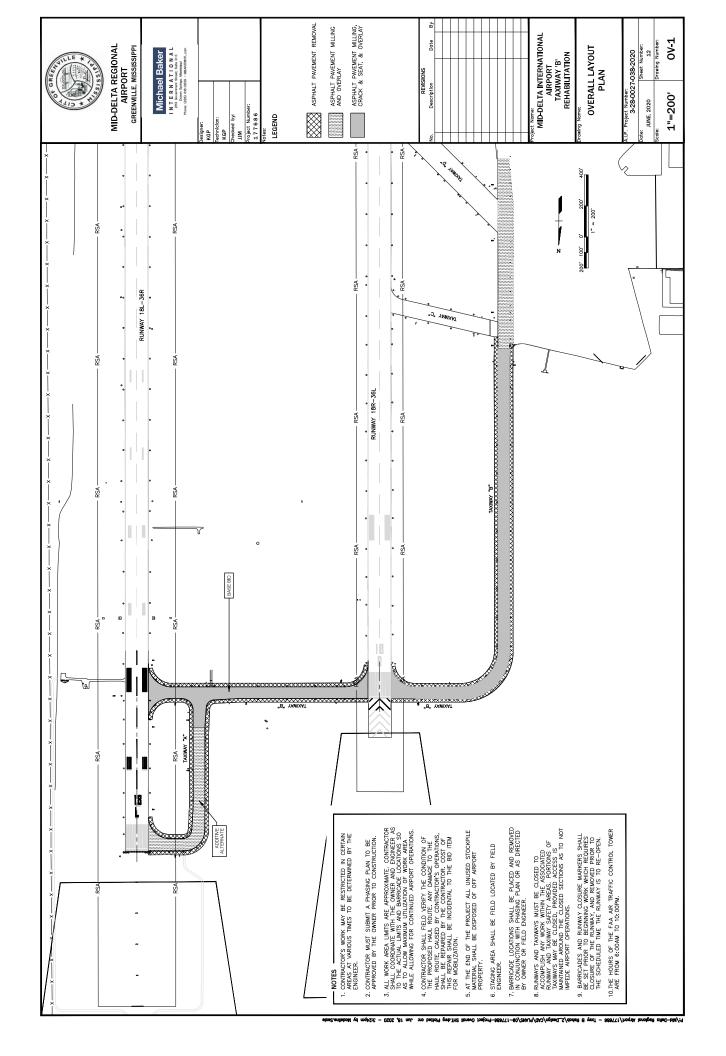


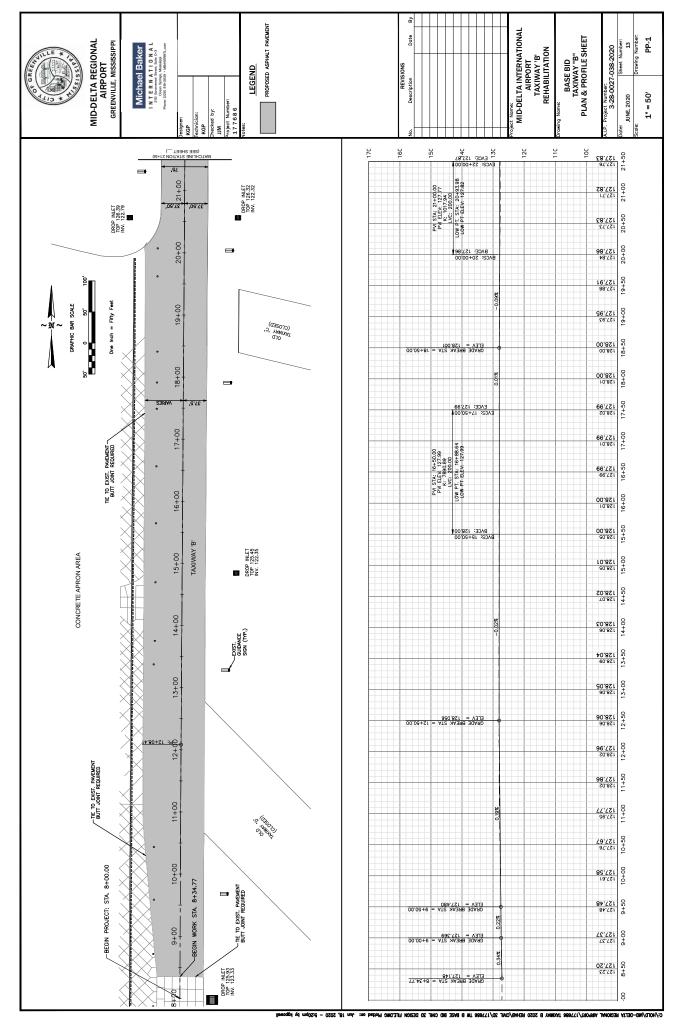
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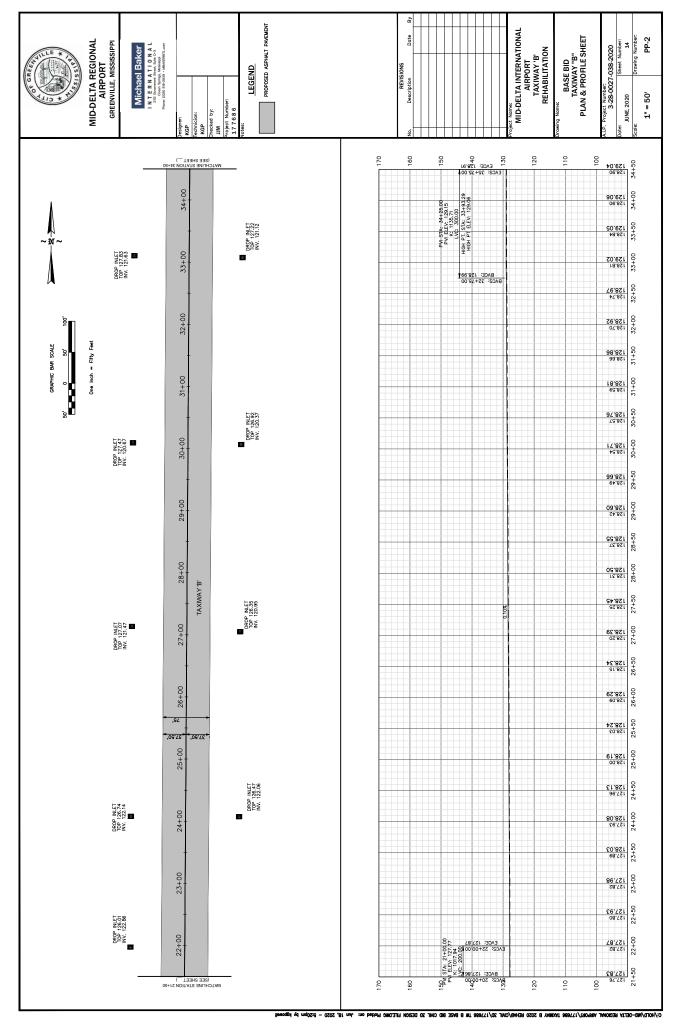


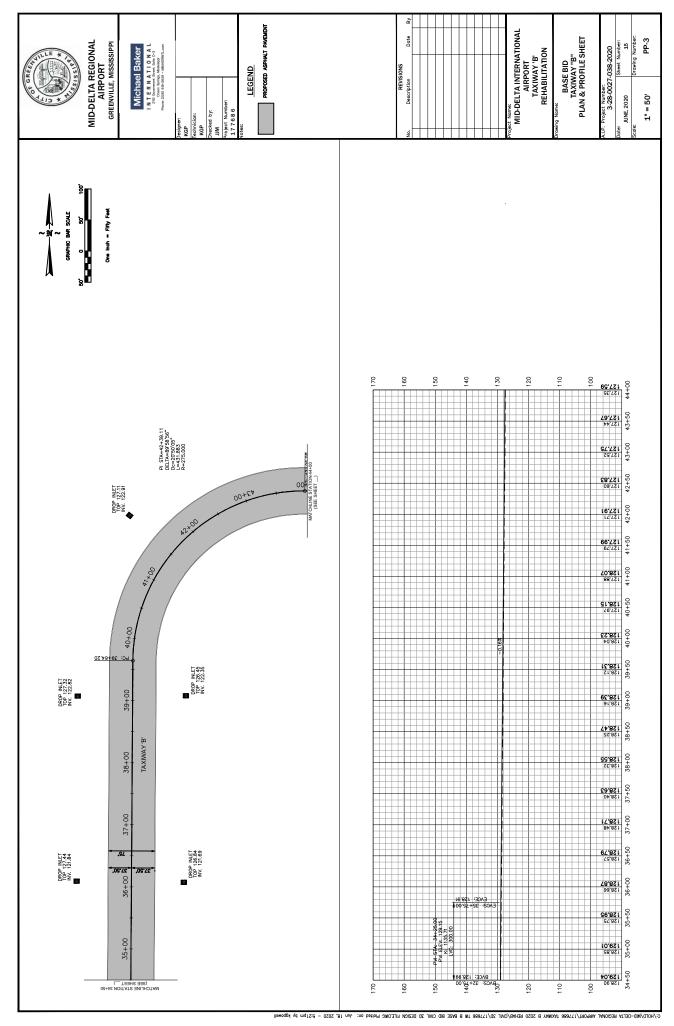


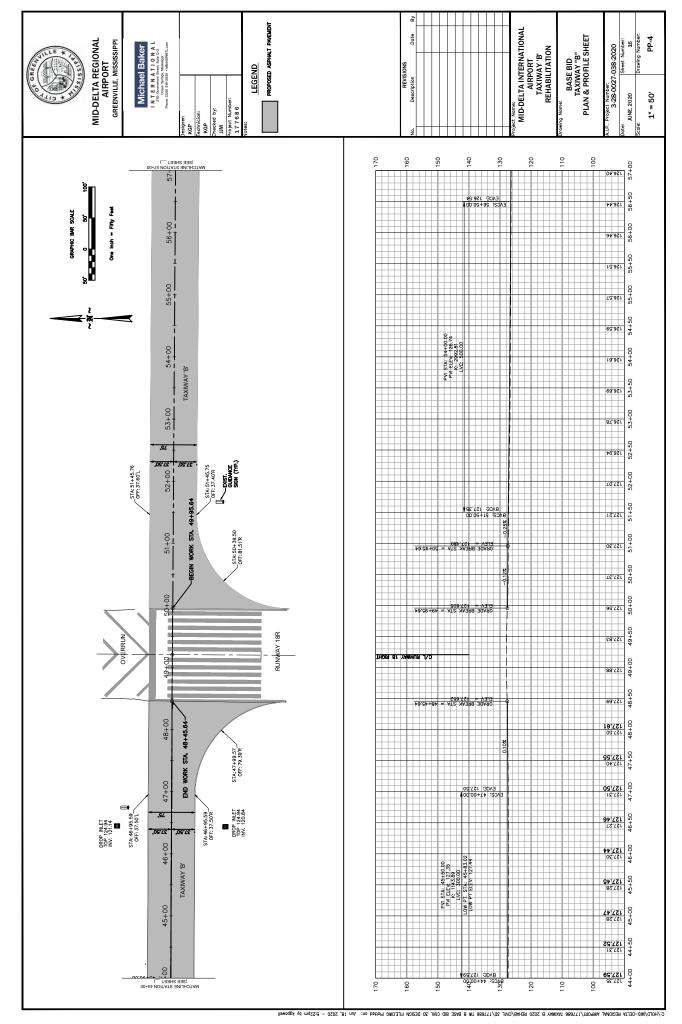


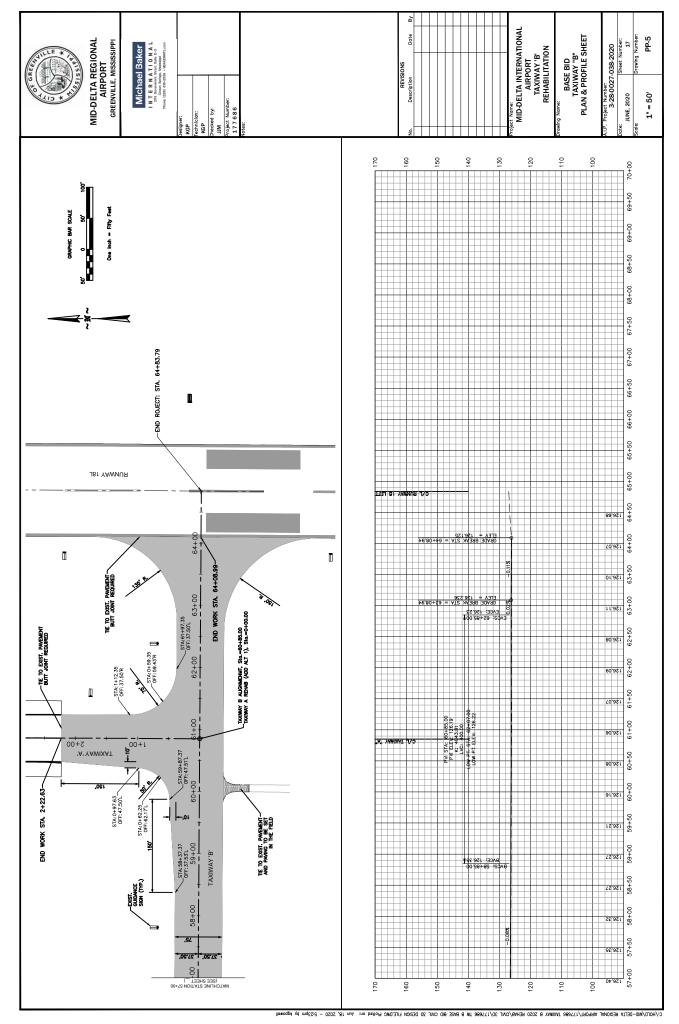


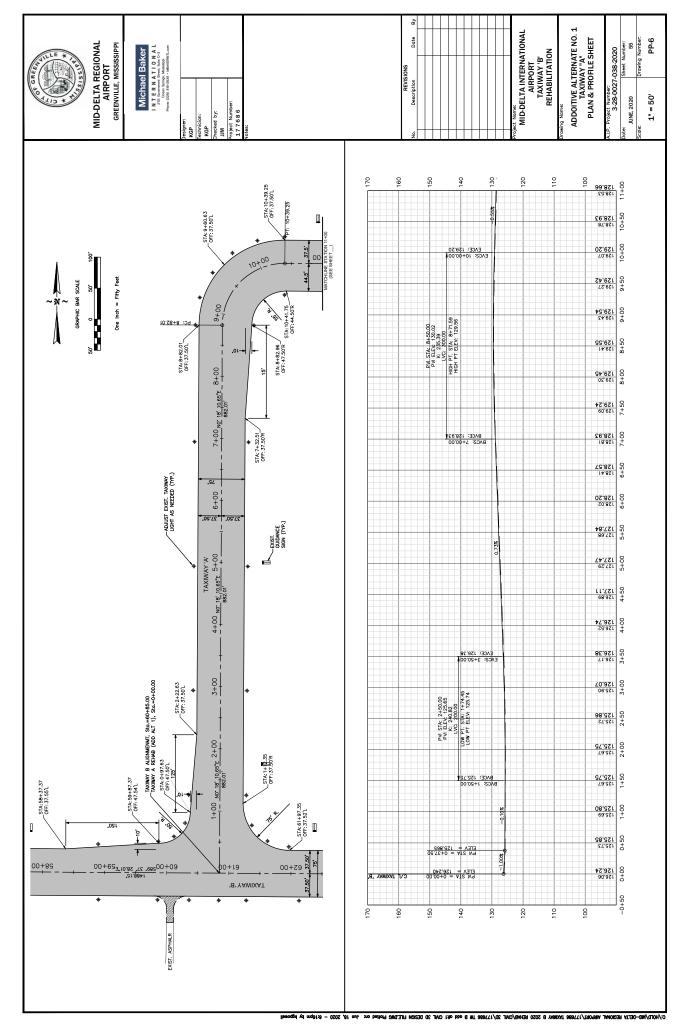


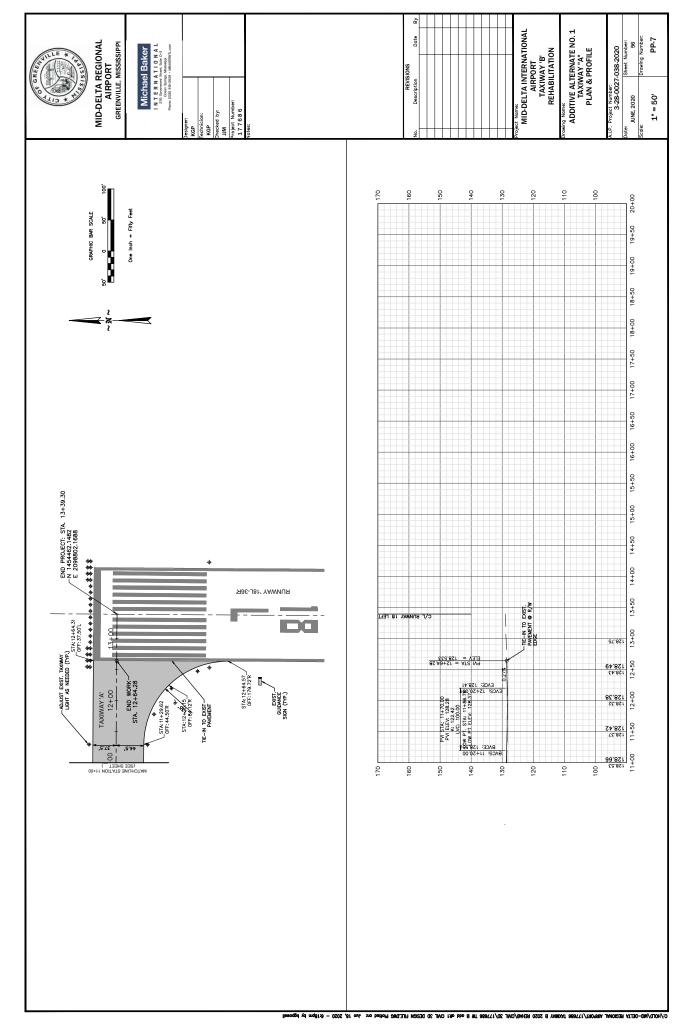


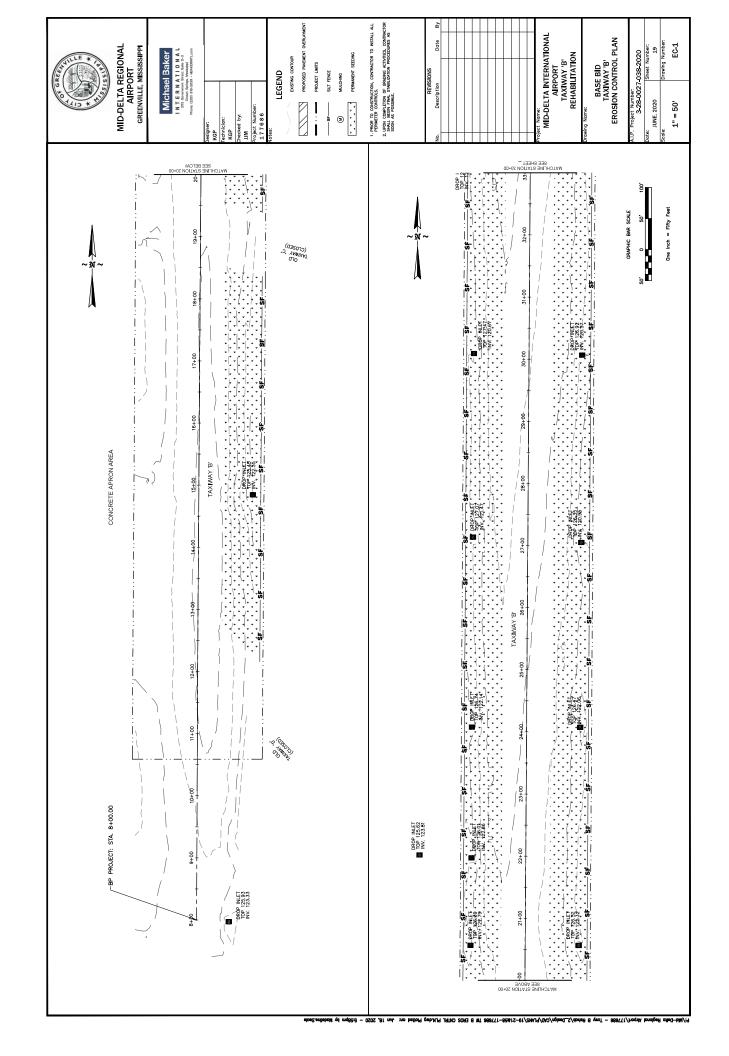


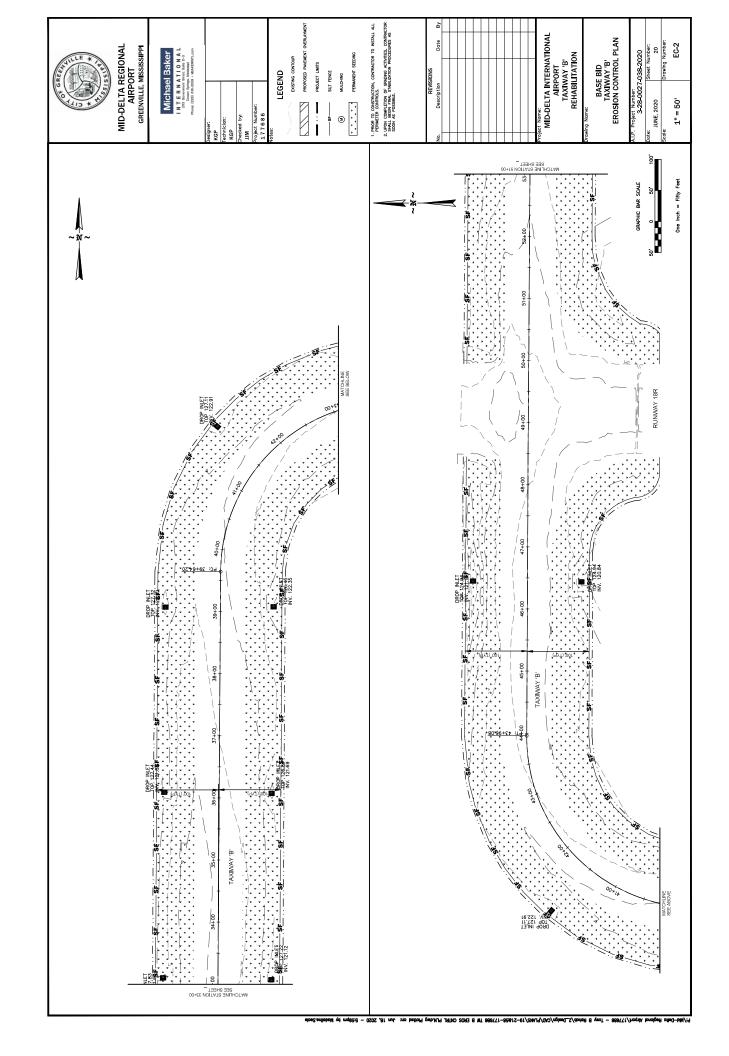


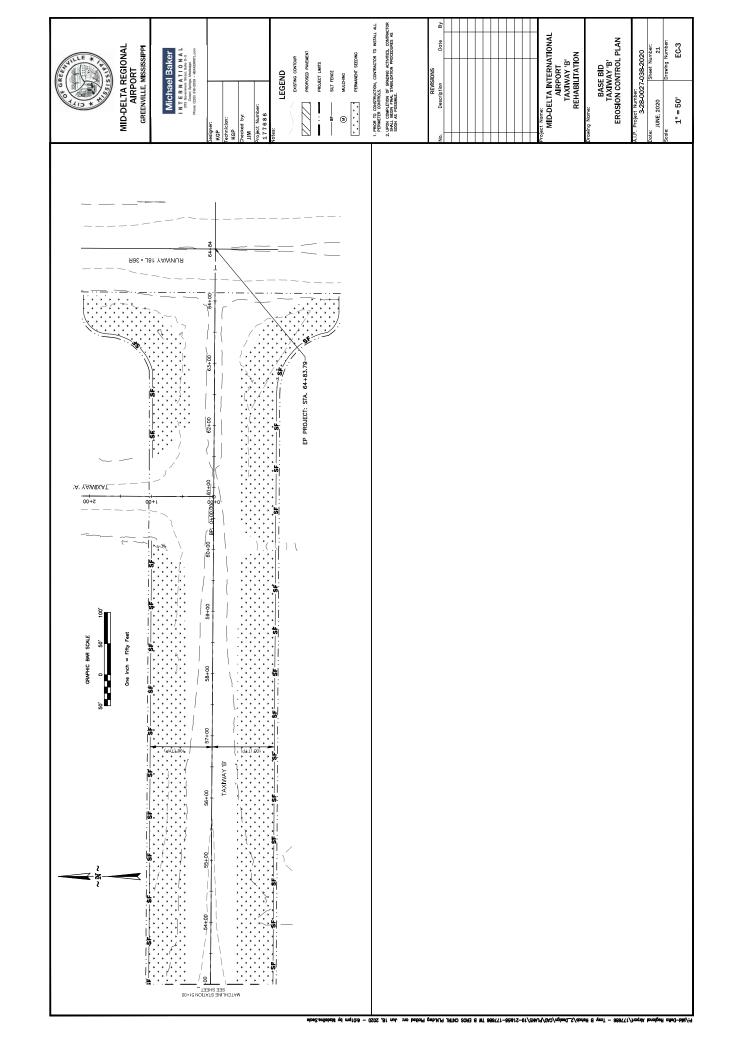


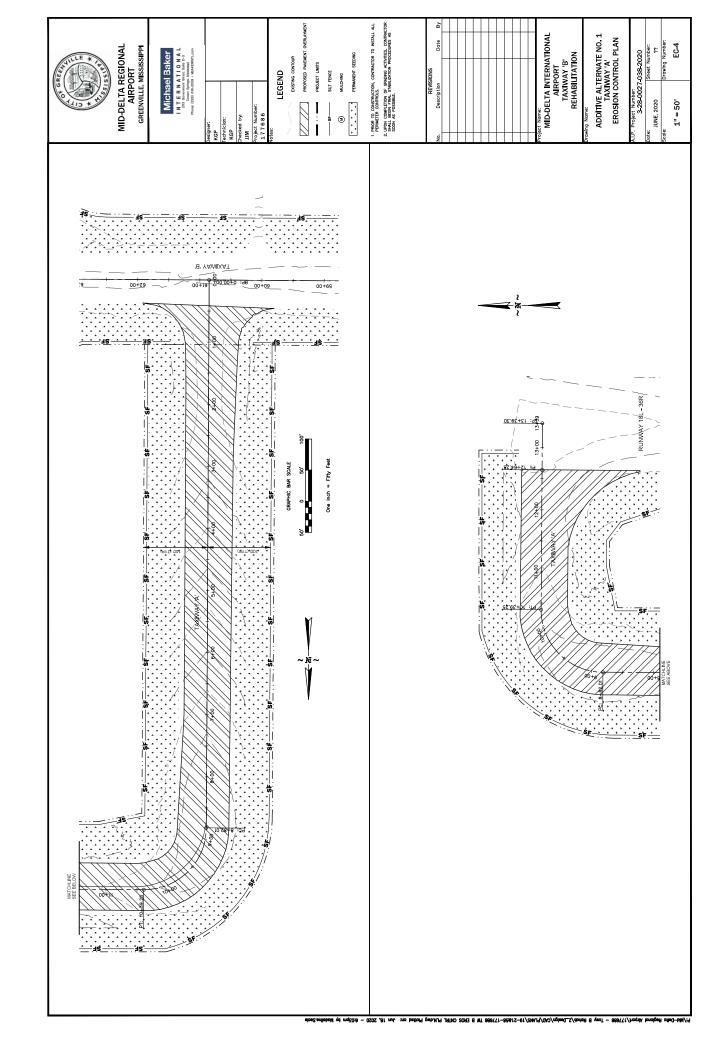
















2. STAPLE WIRE FENCING TO THE POST.

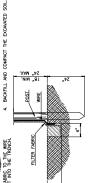
MAXIMUM POST SPACING = 6" WITHOUT WIRE BACKING

MAXIMUM POST SPACING = 10" WITH WIRE BACKING





ATTACH THE FILTER FABRIC TO THE WIRE FENCE AND EXTEND IT INTO THE TRENCH.



FABRIC AND WIRE INTO TRENCH

SILT FENCE DETAIL

WHEN AND WHERE TO USE IT:

SILT FENCE IS APPLICABLE IN AREAS:

MHERE THE MAXIMUM SHEET OR OVERLAND FLOW PATH LENGTH TO THE FENCE IS 100-FEET. WHERE THE MAXIMUM SLOPE SIEEPINESS (NORMAL PREPREDIGUAR) TO FENCE LINE) IS 241Y. HAT TO NOT RECEINE CONCENTRATED FLOMS GREATER THAN 0.5 CFS. DO NOT PLACE SILT FENCE ACROSS CHANNELS OR USE IT AS A VELOCITY CONTROL BMP.

TREE PASTS.

STREET P

USES STEEL POSTS WITH A MUMBAL INCHINE OF HETEN, WEIGHOUT, LZS FOUNDES PER LUEKER FOOT THE PRESSAN FOR THE STEEL PASSES WITH MARKER HETEN, SEED SCREEN HETEN HETEN FOR STEEL STEEL PASSES AND THE PRESSAN FOR THE FREIGHT WHEN THE PASSES WEIGHT STEEL PASSES WITH LANGER A HETEN, SOUNDES STEEL STEEL PASSES THE STEEL STEEL

SOIL PLATES SHOULD HAVE THE FOLLOWING CHARACTERISTICS: COMPOSED OF MINIMUM 15 GAUGE STEEL.

E A MINIMUM CROSS SECTION AREA OF 17—SQUARE INCHES.

SEOTEXTILE FILTER FABRIC:

COPPOSED OF THESE CONSESTING OF USING CHAIN SYNCHETIC POLYMERS COMPOSED OF ALL LIGHT COMPOSED OF THE CHAIN SYNCHETIC POLYMERS COMPOSED OF ALL LIGHT CHAIN STATE CH

JSE GEOTEXTILE FILTER FABRIC THAT CONFORMS TO PROJECT SPECIFICATIONS AND ADEM REQUIREMENTS.

NSTALLATION:

DEADLY THOSE APPROACH TO A THOSE APPROACH AND A THO

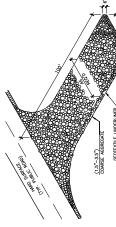
INSPECTION AND MAINTENANCE:

NORPECT ENERY SERVE OLDERING NOX. AND WHITE 24-HOUSE AFTER EACH PRIMELL ENERT THAT PRODUCES. A "HOHER OLDER OF PRECIPITION, CHECK FOR SEDULET BUILDIP AND FENCE THIS ENTER THE BUILDING HAS ENDODE OWNER. BUILDING HOUSE OF OWNER. FINCE WAS SAGED OR COLLASED BY ENCE OVERTOPENIA.

erjore silt fence within 30 days after final stabilization is achieved or after teaporary rests management practices (1949's) are no longer needed. Permanentiv stabilize disturbed teksa fesulting form fence rejoon. IF THE FROE TABLES THERE TABLES TO DECOURSE. OR IN ANY WAS RECOURS INFFECTIVE REPLACE THE SCETON OF FENCE MANEDARTH. RELOVE SEDMENT ACCOULATED ALONG THE FENCE RELOVE. THE RECHEST, AT THE REGUL OF THE FENCE, ESPECIALLY IF HAVY PANS ARE EXPECTED. RELOVES TO STATE THE TABLES SEDMENT FROM THE SITE OF STABLES. If ON STITE.

TEMPORARY SILT FENCE N.T.S.

SHOWN AS ---- SF ON PLANS



1. OAMERONE STEP (1,5-3,5)
1. OAMERONE STEP (1,5-3,5)
2. DAISTONO OF OAMER, PADDITION OF OAMER MANDAM HIGHWAYS OF OAMER MANDAM OF FILL WITH AT ALL POINTS WOTHER MANDAM OF FILL WITH A TALL POINTS WORKER NO DRIT AFEN. WHO

1. LOCATION. LOCATE COMESTICATION BURNACES AND EDITS TO LUMIT SEZIOLAT FINAL MEAN THE SITE NOT PROVIDE FOR MAXIMUM UTILITY BY ALL CONSTRUCTION VEHICLES.

1. LOCATION LOCATION STATE AT CHARGES.

2. LOCATE DID IN PLOCE AT THE BOD OF CONSTRUCTION.

3. LOCATE DID IN PLOCE AT THE BOD OF CONSTRUCTION.

4. LOCATION TO MOBILIZATION.

CRUSHED STONE STABILIZED CONSTRUCTION ENTRANCE

SHOWN AS CO DESCRIPTION PLANS

STABILIZED CONSTRUCTION ENTRANCE

TABLIZED CONSTRUCTION BYTRANCES SHOULD BE USED AT ALL POINTS WHERE TRAFFIC WILL BE SEAVED A CONSTRUCTION SITE AND POINTS OWNED SECTIVE STORY A PUBLIC ROAD.

REMOVE ALL VESENTION AND ANY OBJECTIONNEE MATERIAL PROM THE FOUNDATION JARGE.

SHEART ALL SWADE MAKEN'S AND DEMANGE FROM STUDIES TO A SERMENT TIPPO OR BUSIN.

INSTALL A NOW-WORN GEOTICALE FABRIC PRIOR TO PLACKE ANY STORE.

INSTALL A CLUVERT PIPE ACROSS THE DITHANCE WHEN NEEDED TO PROVIDE POSSITIVE DIRWANGE.

BISTALL A CLUVERT PIPE ACROSS TO THINK THE TO STOVE PLACED AT A MINIMAL DEPTH OF GARRIES.

MINIMUM DIMENSIONS OF THE ENTRANCE SHALL BE 24-FEET WIDE BY 100-FEET LONG, AND MAY BE MODIFIED. A COCOMMODATE SITE CONSTRAINTS.

THE EDGES OF THE ENTRANCE SHALL BE TAPERED OUT TOWARDS THE ROAD TO PREVENT TRACKING. OF MUD AT THE EDGE OF THE ENTRANCE. INSPECTION AND MAINTENANCE: NERECT CONSTRUCTOR RETRACTES EDET SENS IC) CALCIDADE DOS ADMINISTATION RETRACTIONS THE SENS OF SENS AT SENS AT

WASH OR REPACE STONES AS MEDDO AND AS DRECTED BY THE INSPECTOR. THE STONE IN THE REPECTOR. THE STONE IN THE STONE WITH THE PROPERTY OF THE STONE WITH TH MMEDATELY REMOVE MUD AND SEDIMENT TRACKED OR WASHED ONTO PUBLIC ROADS BY BRUSHING SPAREDING. FLUSHING SHOULD ONLY BE USED WHEN THE WATER CAN BE DISCHARGED TO A REMOVENT TRAVE BYSIN. REPAIR ANY BROKEN PAVEMENT IMMEDIATELY.



MID-DELTA REGIONAL AIRPORT GREENVILLE, MISSISSIPPI

Michael Baker
INTERNATIONAL
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TAXIWAY 'B' REHABILITATION

EROSION CONTROL

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SPECIFICATIONS PERMANENT GRASSING GRASSING NOTES: TEMPORARY GRASSING

APPLY TEMPORARY GRASSING TO DISTURBED AREAS WITHIN 14 DAYS OF LAND DISTURBING ACTIVITIES IF PERMANENT GRASSING IS NOT REQUIRED. APPLY TEMPORARY GRASSING TO SOIL STOCKPILES.

APPLY SEED MIXTURE AT SPECIFIED APPLICATION RATE EVENUX. NO NOT SEED BAY AREA IN ENESS OF THAT WHICH CAN BE MULCHED ON THE SAME DAY. DO NOT SOW MIMEDIATLY COLUMNA A RAIN, WHEN GROUND IS TOO DRY, OR DURING WINDY PERIODS. ALL AREAS RECEIVING TEMPORARY GRASS MIXTURE SHALL RECEIVE AN APPLICATION OF FERTILIZER AND BE PROTECTED WITH MULCH. 7 m

MULCH SHALL BE APPLIED IMMEDIATELY AFTER GRASSING. ALL MULCH SHALL RECEIVE AN APPLICATION OF TACKIFIER.

TACKIFIER IS TO BE APPLIED AS PER MANUFACTURERS RECOMMENDATIONS. Ġ.

TEMPORARY MIXTURES ACCEPTABLE FOR THIS PROJECT ARE LISTED IN THE SPECIFICATIONS, SEED MIXTURES GROW RAPIDLY AND ARE LOW-MAINTENANCE.

ARE COMPLETE AND ALL ADDITIONS ARE CRADING OPERATIONS ARE COMPLETE AND ALL ADDITIONAL CONSTRUCTION OPERATIONS WILL NOT INFACT THE DISTURBED AREA. APPLY DEPARAMENT GRASSING TO ALL NON-CONSTRUCTION AREAS WHICH SHOW SIGNS OF EXCESSIVE ERGSION.

ALL AREAS RECEIVING PERMANENT GRASS MIXTURE SHALL RECEIVED AN PAULCATION OF FERRILIZER AND BE PROFICETED WITH MILCH AND/OR REOSION CONTROL MATING, APPLY UNIT A RATE BASEO NP HOT SOIL, FOR DAY CRASSING, UNITORIALY INCORPORATE INTO SOIL, FOR A DEPTH OF 1 INCH AND LIGHTLY WATER TO AND THE DISSIPATION OF FERRILIZER.

APPLY SED MIXTURE AT SPECIFED RATES EVENLY IN TWO INTERCENTION BURECHOINES OF MECHANICAL SPREADER OR HYDROSEDER. DO NOT SEED AM AREN IN DECESS OF THAT WHICH CAN BE MULLCHED ON THE SAME DAY. DO NOT SOW IMMEDIATELY FOLLOWING A FAIN, WHEN GROUND IS TOO DRY, OR DURING WINDY PERROA S. RAIN, WHEN GROUND IS TOO DRY, OR DURING WINDY PERROA S.

COMBINED HYDRAULIC APPLICATION OF SEED, FERTILIZER, AND MUCH HAN EF PERFORMED, TACKIFER APPLICATION MAY BE WITHIN THE COMBINED MIXTURE IF ALLOWED BY MUMACATURER'S RECOMMENDATIONS, HYDRAULIC SPRAYING EQUIPMENT AND MIXTURE SHALL BE DESIGNED SUCH THAT MIXTURE SHALL BE DESIGNED SUCH THAT MIXTURE COMPONENTS SHALL BE EQUAL IN QUANTITY TO THE SPECIFIED RATES.

WOOD FIERS MULCH SHALL BE APPLED AS INDICATED IN THE PROJECT SPECIFORMINS. STREM MILCHS SHALL BE UTILIZED TO PREVADE. A UNIFORM LOSSE DEPTH BETWEN 1-1/2 TO 3 TACKFIER.

TACKFIER.

PERMANENT MIXTURES ACCEPTABLE FOR THIS PROJECT ARE LISTED IN THE SPECIFICATIONS. THESE MIXTURES ARE GROW RAPIDLY AND ARE LOW-MAINTENANCE.

GRASSING SCHEDULE:

HOROSEEDING IS THE WET HYDRAULIC SPRAYING OF SEED, PROCESS, MATERIALS AND USDULLY MULLON IN A DIG-SEED PROCESS, MATERIALS ARE MIXED WITH WHITER IN A SULREY MATERIAL TO FORM A HOMOGENEOUS SULREY, WHICH IS THEN SPRAYED ON THE SOIL SURFACE BY A HYDRAULIC SEEDER.

MID-DELTA REGIONAL AIRPORT GREENVILLE, MISSISSIPPI

REFER TO THE PROJECT SPECIFICATION T-901 FOR ADDITIONAL INFORMATION.

ORDINARY WULCH IS NOT SUITABLE FOR HYDROSEEDING. MULCH FOR HYDROSEEDING SERREALLY WOOD FBER WULCH, WANUFACTURED TO BE UNIFORMY. SUSPENDED AS A HYDROSEEDING. BALL BE APPUED AFTER THYDROSEEDING.

MAINTENANCE

5

CHECK FOR DAMAGE CAUSED BY EQUIPMENT OR HEAVY RAINS. DAMAGED RAFAS SHOULD BE REPAIRED, FERTILIZED, SEEDED, AND MULCHED, TACK OR TIE DOWN MULCH AS NECESSARY TO PREVENT BLOWING. ń

AT THE TIME THAT A SATISFACTORY VEGETATION HAS BEEN ASSENSISHEND, LITE INFORMORY OFFICIAL BET REMOVED FROM THE SITE, AND LA RETROSTITE BE DESTINGN PROSON WILL BE CLEANED AND GRADED TO THE DETENTION POND DESIGN.

7.

hecked by: KGP

INTERNATIONAL
2113 Government Stroet, Suite D-3
Ocean Springs, Mestastipii
Phone (228) 819-2839 • MBAKERPITL.com

Michael Baker

Project Number: 177686

Date REVISIONS

MID-DELTA INTERNATIONAL AIRPORT TAXIWAY 'B' REHABILITATION

EROSION CONTROL DETAILS

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HYDROSEEDING

INSPECT FREQUENTLY WITHIN THE FIRST SIX WEEKS OF PLANING TO SEE INCHESIAN ARE UNIVERNA NO EDENSE, AND TO ASSURE THAT APPROPRIATE MOISTURE LIVELS ARE MAINTAINED. MAKE PROVISIONS TO WATER AS NEEDED.

CONTRACTOR TO REVISE AND MULCH AS NECESSARY UNTIL THE GRASS IS SATISFACTORY TO THE OWNER.

ADDITIONAL VEGETATION WILL BE EMPLOYED WHERE DETERMINED NECESSARY BY ACTUAL SITE CONDITIONS AT CONTRACTOR EXPENSE.

4

GRASS THAT ATTRACTS WILDLIFE SHALL NOT BE USED. 'n e.

ALL TEMPORARY EROSION AND SEDIMENT CONTROL DEVICE MAINTENANCE, REMOVAL, REPAIR, AND RECONSTRUCTION COSTS WILL BE INCIDENTAL TO THE COST OF THE PROJECT.

AUGUST SEPTEMBER OCTOBER NOVEMBER DECEMBER JULY TEMPORARY GRASSING SCHEDULE JUNE MAY APRIL MARCH FEBRUARY JANUARY ANNUAL RYE GRASS (50#/AC) FERTILIZER 10-10-10 (100#/AC)

ingeloud kirport/177686 – Txwy B Reinob/Z⊾Deelgn/CAD/PLANS/69-70-177696 Eros crint DET-1.dwg Protted on: Jun 18, 2020 – 6:20pm by Mac

NOVEMBER DECEMBER OCTOBER SEPTEMBER AUGUST JULY PERMANENT GRASSING SCHEDULE JUNE TO BE APPLIED TO GRASS BETWEEN 2" AND 4" IN HEIGHT (TOPDRESSED). TEMPORARY GRASSING ONLY FROM NOVEMBER 16TH TO FEBRUARY 15TH APRIL MARCH FEBRUARY JANUARY COMMON BERMUDA (UNHULLED-50#AC) COMMON BERMUDA (HULLED-50#/AC) FERTILIZER 10-10-13 (1000#/AC) AGRICULTURAL LINE (3000#/AC) AMMONIUM NITRATE (50#/AC) -34-0-0 TALL FESCUE (100#/AC) BERMUDA (100#/Ac)



Appendix D
United States Geological (USGS)
Quadrangle Map

